

a.2 Accent Paving Standards

- Contrasting paving materials, textures and/or colors used at the entries to create a sense of arrival
 - Contrasts in these paving materials shall be subtle, such as a change in color or patterns, and enhance the project entry scenes
 - Accent paving patterns and joints shall work intricately with any proposed and existing tree grates and landscape areas
 - Accent paving may consist of concrete pavers, integral colored concrete with stamped or textured finish, stone, brick, or cobbles
- Accent paving may combine harmonious paving materials to develop a beautiful and unique entry treatment
 - Primary Gateways shall allow for larger expanses of accent paving at the entry corners if feasible
 - Accent paving shall be permeable where feasible



b. PRIMARY GATEWAY ENTRY



Accent Paving
Examples



b. PRIMARY GATEWAY ENTRY

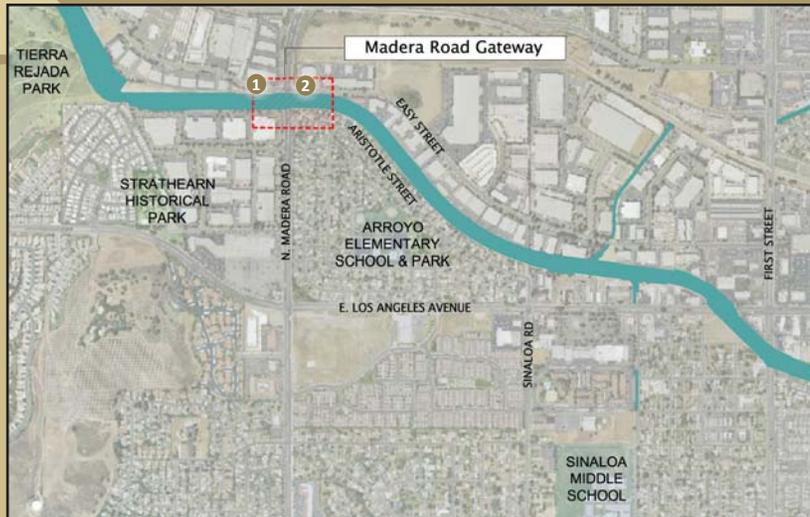
a.3 Madera Road Gateway

Madera Road Gateway is located on a major arterial and the road to the Ronald Reagan Library, and is considered a “Welcome Corner” into the City. The Arroyo Simi Greenway is proposed to connect with the City of Moorpark and Tierra Rejada Park, but this Gateway may be the trail terminus for several years.

This entry has considerable potential and is in need of improvements. There is currently a small existing rest area and many existing trees, but the gates appear locked from the intersection and it is not inviting to potential trail users.



View of intersection at Madera and Los Angeles looking east



Madera Road Gateway Location

View looking west towards Madera Road



There was an under crossing proposed for this intersection in the 2007 Visioning Study, but upon further study with the signalized at grade crossing and steep rock embankments and a low bridge it does not appear as feasible to construct this under crossing. Therefore, a new pedestrian/bicycle bridge across the Arroyo to the west of Madera Road is proposed to connect with the Tierra Rejada Trailhead. (Refer to *Bridges, Section 2.6* for additional information)

The Madera Road Gateway is in an industrial business section of the Greenway, and the inclusion of a rest stop in the gateway scene could provide workers with a lunch area within walking distance of their place of employment, and offer trail users a destination and meeting spot.



Improve the existing rest stop to provide a destination, meeting spot and lunch area

Madera Road Gateway shall have all items listed in the Project Entry Standards in Section 2.11.1, and Primary Gateway Standards in Section 2.11a.1, plus:

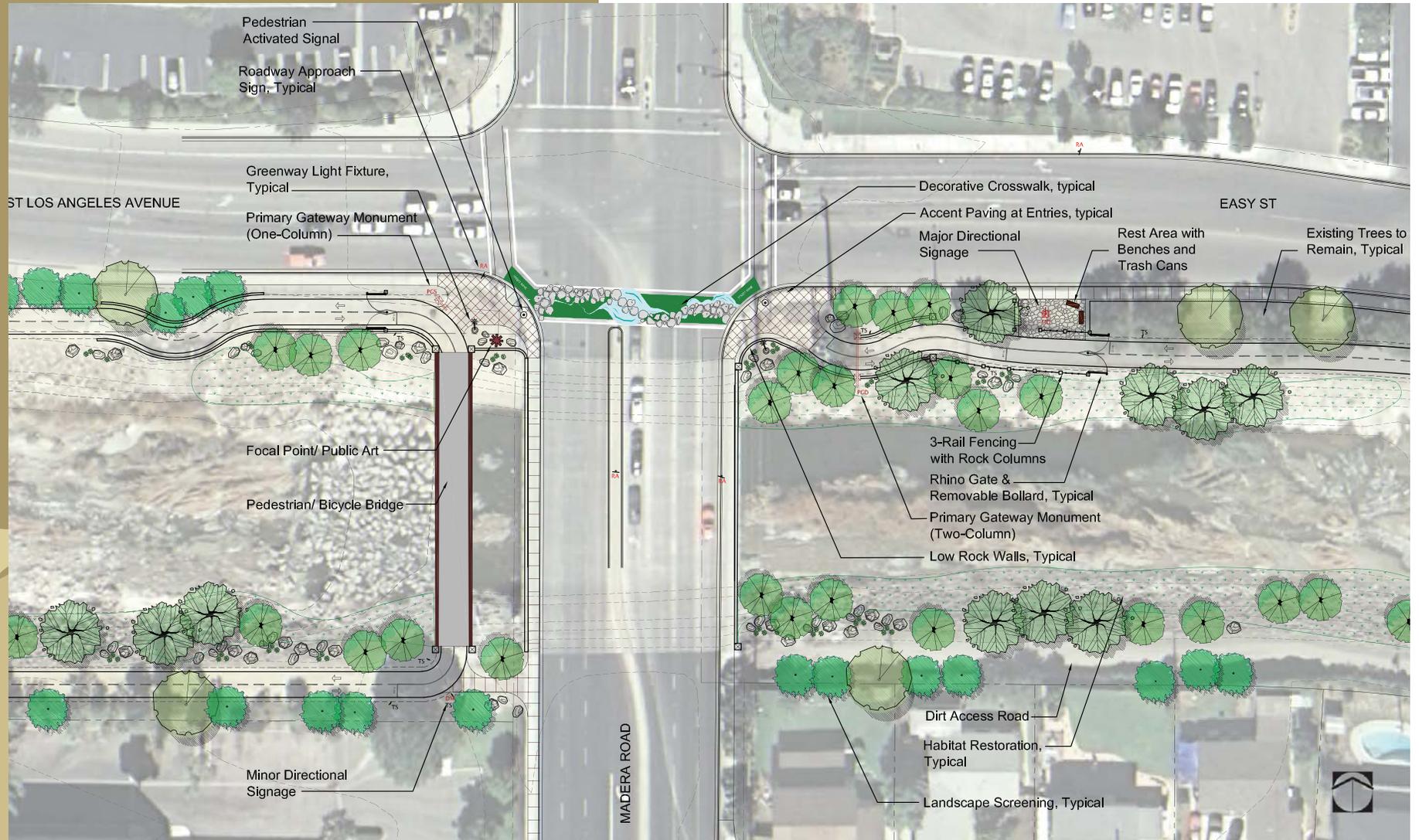
- Existing Rest Area enhancement (refer to *Overlooks/Rest Areas, Section 2.9a*)
- Major Directional Signage (refer to *Greenway Signage Program, Section 2.12i*)
- Pedestrian/Bicycle Bridge (refer to *Bridges, Section 2.6c*)



Madera Road Gateway plan view



b. PRIMARY GATEWAY ENTRY



Madera Road Gateway
view looking east



b. PRIMARY GATEWAY ENTRY



b. PRIMARY GATEWAY ENTRY

a.4 Erringer Road Gateway

Erringer Road Gateway is a portion of the Key Study Area for Rancho Simi Community Park Expansion and is located on a major arterial, Erringer Road. There are currently many children and teenagers using this park as a pathway to surrounding schools.

The south side entry is along the primary trail alignment, and it is located directly adjacent to Rancho Simi Community Park. This proximity to the park allows for many connection possibilities and there is ample room and design flexibility for the entry.



Erringer Road Gateway Location

View of the south side entry at Rancho Simi Community Park looking west



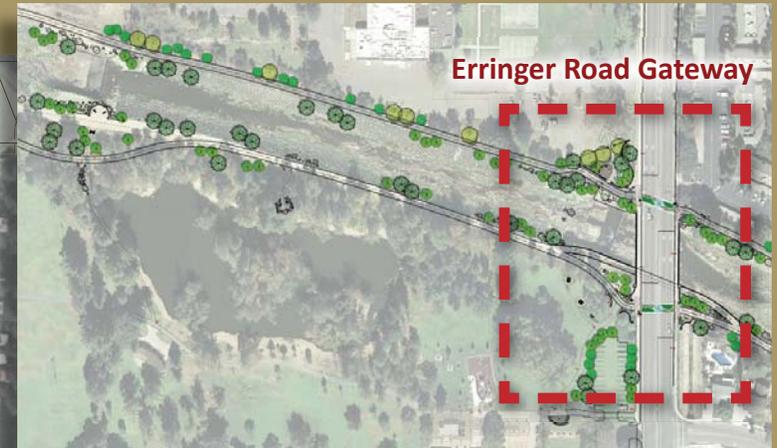
The entry on the north side is currently owned by the City of Simi Valley, and it is located on the secondary trail alignment. This entry offers the potential for an attractive rest stop with native plants and gateway enhancements.

Refer to Overlooks/Rest Areas, Section 2.9a for additional information



View of north side entry at area for the proposed overlook/rest area looking northwest from Erringer Road

Erringer Road Gateway plan view



b. PRIMARY GATEWAY ENTRY

Erringer Road Gateway is a portion of the Key Study Area for Rancho Simi Community Park Expansion

Erringer Road Gateway shall have all items listed in the Project Entry Standards in Section 2.11.1, and Primary Gateway Standards in Section 2.11a.1, plus:

- Pathway from the south entry into Rancho Simi Community Park, with the rhino gates located behind this pathway connection

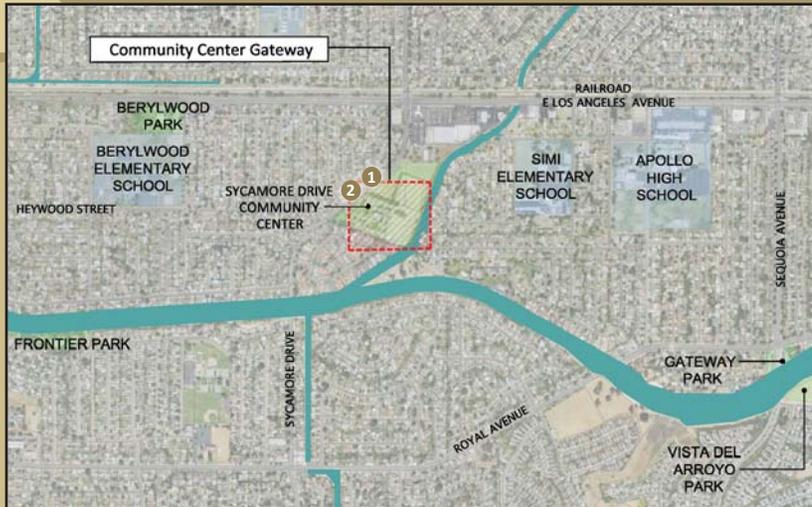


b. PRIMARY GATEWAY ENTRY

a.5 Community Center Gateway

The Community Center Gateway is a portion of the Key Study Area for the Sycamore Drive Community Center Trailhead, located on a major arterial. Sycamore Drive intersects with Heywood Street and has the potential to become a Class 3 link with Berylwood Elementary School and Park, and is a portion of the Cross-town Route 3 proposed by the Simi Valley Bicycle Master Plan, dated December 2008.

Potential realignment of the existing parking lot and driveway entrances could provide for a gateway plaza that does not require driveway crossings to enter the Greenway. (Refer to Trailheads/ Parking Section 2.4 for additional information)



Community Center Gateway Location



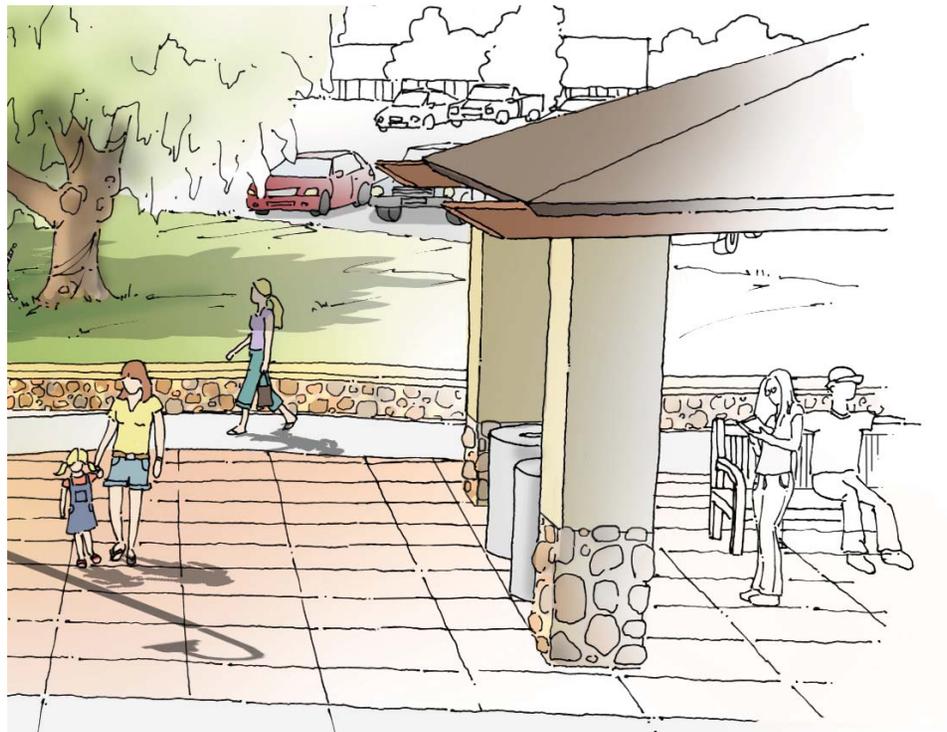
1 This group of photos shows the view looking east from Sycamore Drive at existing parking area and California Pepper Trees to remain



2 View looking south to existing sidewalk, driveway entrance and Sycamores trees to remain

The Community Center Gateway shall have all items listed in the Project Entry Standards in Section 2.11.1, and Primary Gateway Standards in Section 2.11a.1, plus:

- Bus stop enhancements with a Craftsman style bus shelter to match other Greenway buildings and relocation of existing bus stop (refer to Section 1.3c)
- Connection into Trailhead parking area (refer to Trailheads/Parking Section 2.4 for additional information)
- Widened sidewalk to connect to Heywood Street (refer to Trails, Section 2.5f)
- Information Trailhead Kiosk (refer to Greenway Signage Program, Section 2.12c)
- Bike racks (refer to Site Furnishing Program, Section 2.14b)
- Focal point/public art (refer to Section 2.15, Focal Points/Public Art Opportunities)



Gateway bus stop enhancement



Examples of gateway focal point/public art possibilities



b. PRIMARY GATEWAY ENTRY

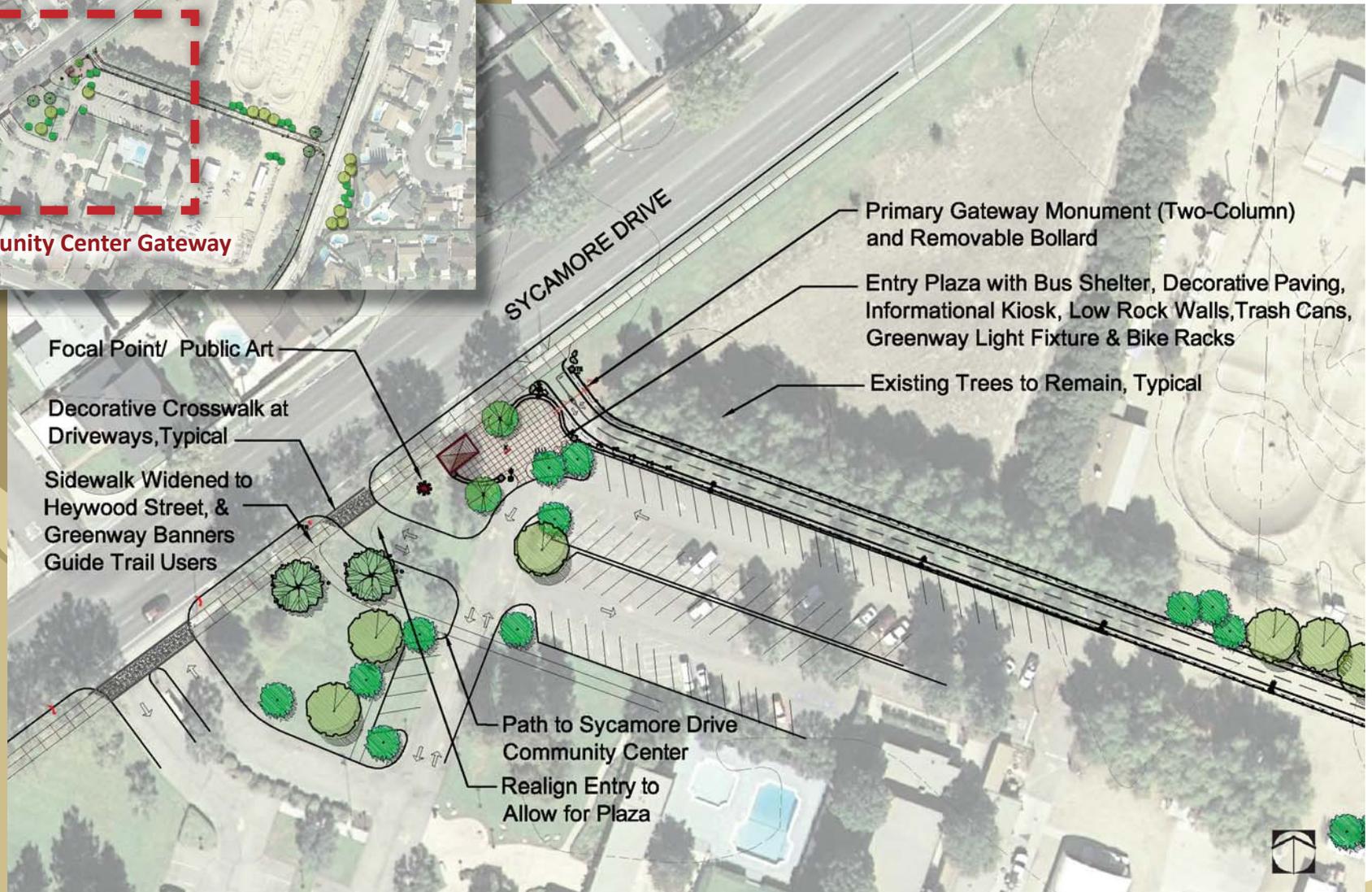


Community Center Gateway is a portion of the Key Study Area for Sycamore Drive Community Center Trailhead



Community Center Gateway

Community Center Gateway plan view



Community Center Gateway
view looking east



b. PRIMARY GATEWAY ENTRY



b. PRIMARY GATEWAY ENTRY

a.6 S-Curve Park Gateway

The S-Curve Park Gateway is a portion of the Key Study Area for the S-Curve Pocket Park, and is located on a major arterial. This gateway has surrounding connections to the existing Kadota Fig Trail to the north and the Amtrak/Metrolink Station and proposed Trailhead at Santa Susana Community Park to the east.

The entry is very visible from Los Angeles Avenue and offers the potential for highlighting the Arroyo Simi Greenway to many people. The Primary Gateway Monument should be placed where it is easily discernible from the roadway and clearly identifies the trail entry.

Looking south



1 This group of photos shows the existing entry gates and fencing



Looking southeast



2 View of existing entry from corner of Angus Avenue and Los Angeles Avenue looking east



S-Curve Pocket Park Location

S-Curve Gateway shall have all items listed in the Project Entry Standards in Section 2.11.1, and Primary Gateway Standards in Section 2.11a.1, plus:

- Include bicycle roundabouts to allow for smooth flowing trail traffic through the gateway entrance (refer to Trails, Section 2.5)
- Major Directional Signage (refer to Section 2.12i, Greenway Signage Program)
- Greenway banners (refer to Section 2.12g, Greenway Signage Program)
- Focal point/public art (refer to Section 2.15, Focal Points/Public Art Opportunities)



Roundabouts could incorporate accent paving, focal points/public art, and/or planting to soften the hardscape and provide visual interest



Provide roundabouts to allow for smooth trail intersections and to slow down bicyclists



b. PRIMARY GATEWAY ENTRY



b. PRIMARY GATEWAY ENTRY

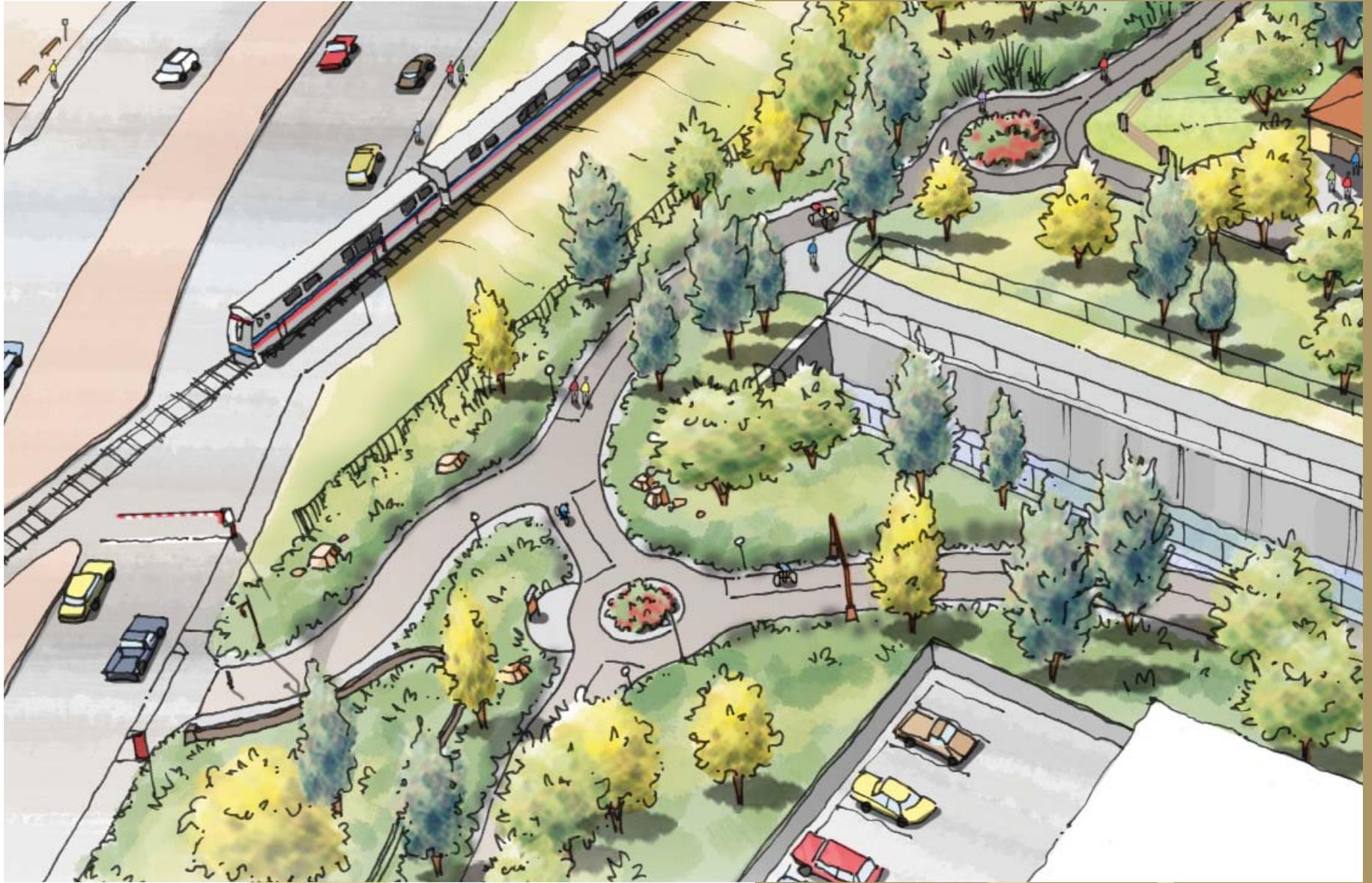


S-Curve Park Gateway
plan view

S-Curve Park Gateway is a portion of the Key Study Area for S-Curve Pocket Park adjacent to Los Angeles Avenue



S-Curve Park Gateway
view looking east



b. PRIMARY GATEWAY ENTRY



b. Secondary Gateway Entry

Multiple Secondary Gateway Entries are located throughout the 12-mile long Greenway, and there are several options available for Secondary Gateway designs to allow for flexibility to arrange according to the space availability and the desired entry experience.

If maintenance and/or emergency vehicle access is available at alternative locations, a split trail could be used to create a more natural entry which still blocks vehicular access.



Secondary Gateway entries could be a entry plaza if space allows (example from Easy Street Gateway)



Secondary Gateway entries could incorporate a split trail if vehicular access is available close by (example from Darrah Volunteer Park Gateway)

Locations:

- Moorpark and Simi Valley City Limits Gateway
- Wastewater Treatment Plant Easement Gateway
- Easy Street Gateway (Portion of Key Study Area 3, refer to Parks, Section 2.8a)
- Fifth Street Gateway (Portion of Key Study Area 4, refer to Trails, Section 2.5e)
- Los Angeles Avenue Gateway
- First Street Gateway
- Sycamore Drive Bridge Gateway (Portion of Key Study Area 8, Refer to Bridges, Section 2.16c)
- Existing Drainage Trail Connection by Community Center
- Royal Avenue Gateway
- Darrah Volunteer Park Gateway (Portion of Key Study Area 11, Refer to Parks, Section 2.8b)
- Tapo Canyon Road Gateway
- Tapo Street Gateway
- Stearns Street Gateway
- Stow Street Gateway
- Yosemite Avenue Gateway (Key Study Area 13)
- Katherine Road Gateway

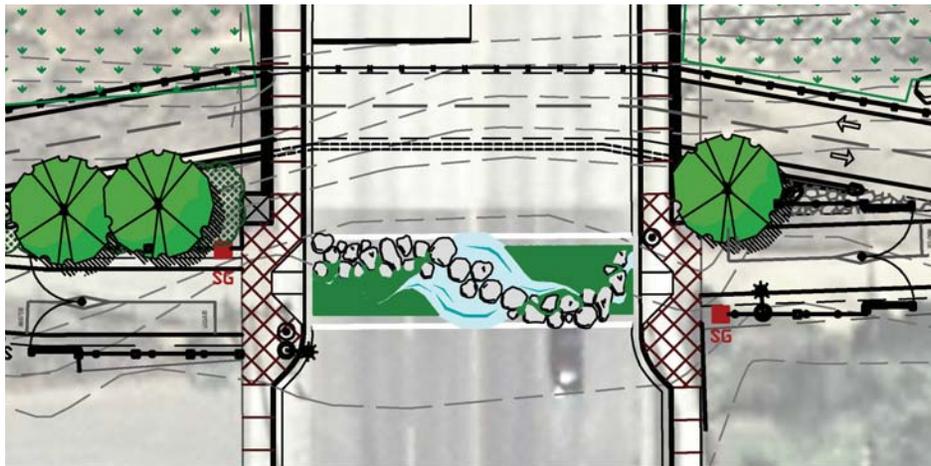


Locations of Secondary Gateway Entries

b.1 Secondary Gateway Standards

Secondary gateway entries shall include all items listed for Project Entry Standards in Section 2.11.1, plus:

- Secondary Gateway Monument (*refer to Greenway Signage Program, Section 2.12e*)
- Provide accent paving along the existing sidewalk where feasible (*refer to Section 2.11 a.2*)



Secondary Gateway entries should provide accent paving along the existing sidewalk at entries (example from Sycamore Drive Bridge Gateway)



Proposed Secondary Gateway Monument (refer to Section 2.12e)





b.2 Yosemite Avenue Gateway

Yosemite Avenue Gateway is a Key Study Area, and located along a collector street. Depending on phasing and funding, the west side of Yosemite Avenue is likely to be the terminus of the Arroyo Simi Greenway for several years.

The east side has some right-of-way, ownership and structural engineering issues, and two alternatives have been developed for this section of trail. One option has the trail crossing over the Arroyo Simi on a platform or bridge, and allows for Major Directional Signage and landscaping south of the trail. Another option narrows the trail to 6 feet at the entry, and relocates the Major Directional Signage to the west side of the road.



Yosemite Avenue Gateway Location

Yosemite Avenue Gateway shall have all items listed in the Project Entry Standards in Section 2.11.1, and Secondary Gateway Standards in Section 2.11b.1, plus:

- Chain link fencing retrofit improvements (*refer to Fences and Walls, Section 2.13a.4*)
- Provide new entry fences along Yosemite Avenue (*see Fences and Walls, Section 2.13a*)
- Major Directional Signage (*refer to Greenway Signage Program, Section 2.12i*)
- Entry to the east shall maintain private property access



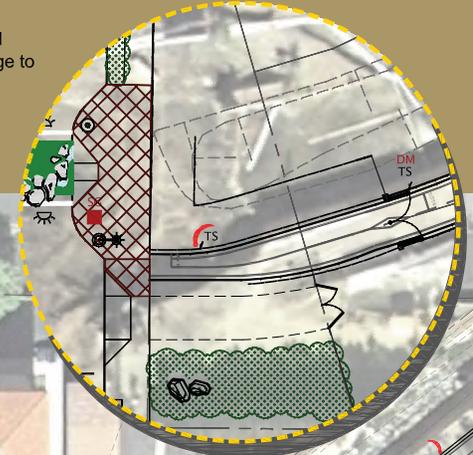
View of existing paved trail looking west



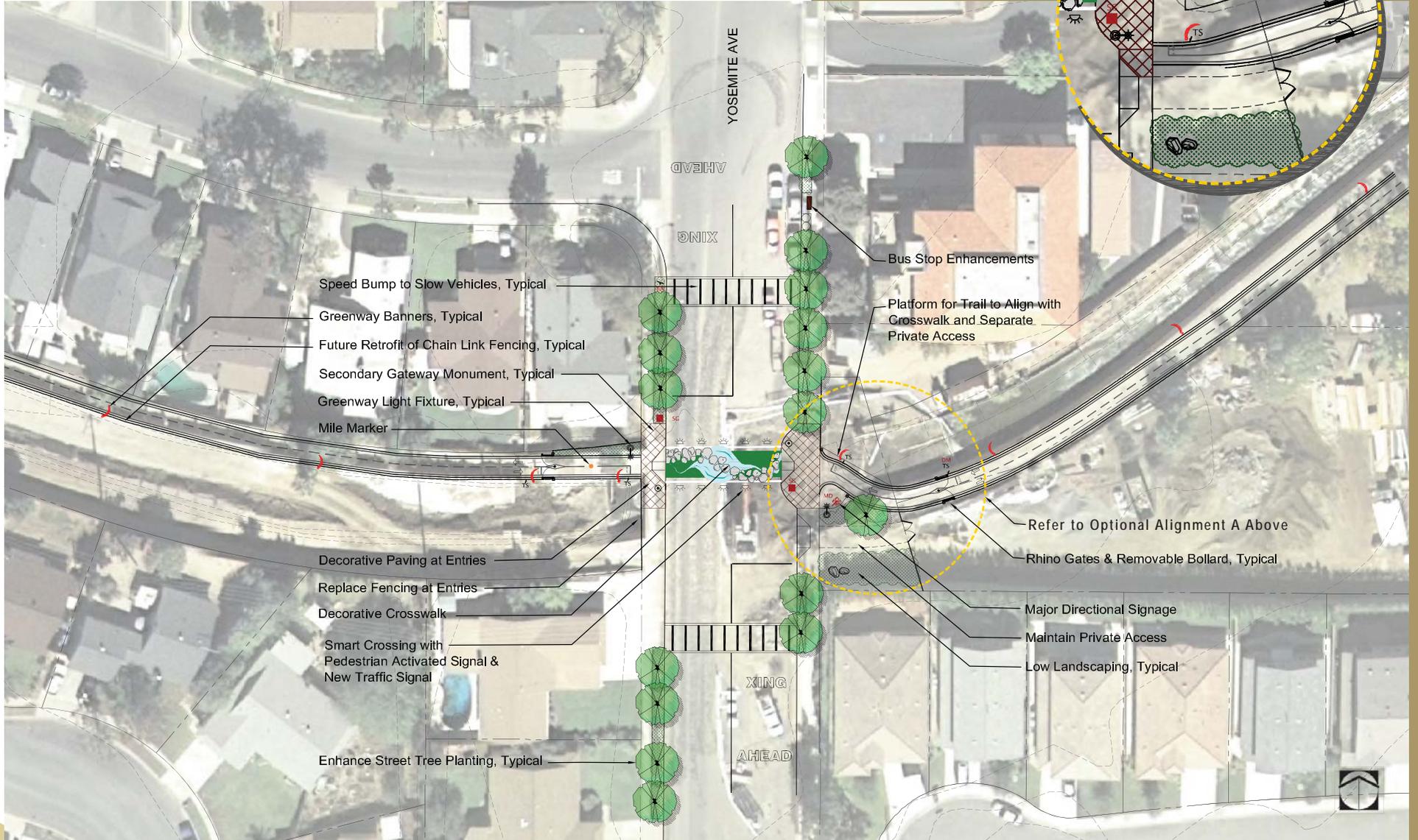
View of existing entry and residential access looking east

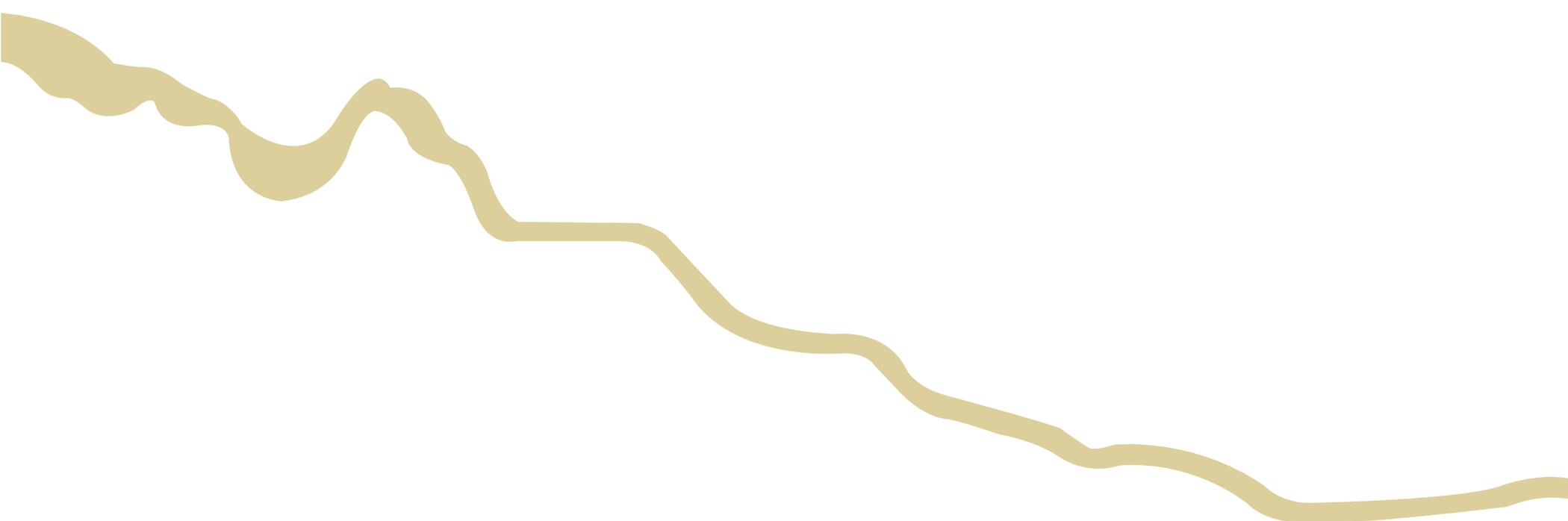
Yosemite Avenue Gateway plan view

OPTIONAL ALIGNMENT A
Narrows Trail at Entry to 6' and
Relocates Major Directional Signage to
Opposite Side of the Road



a. SECONDARY GATEWAY ENTRY

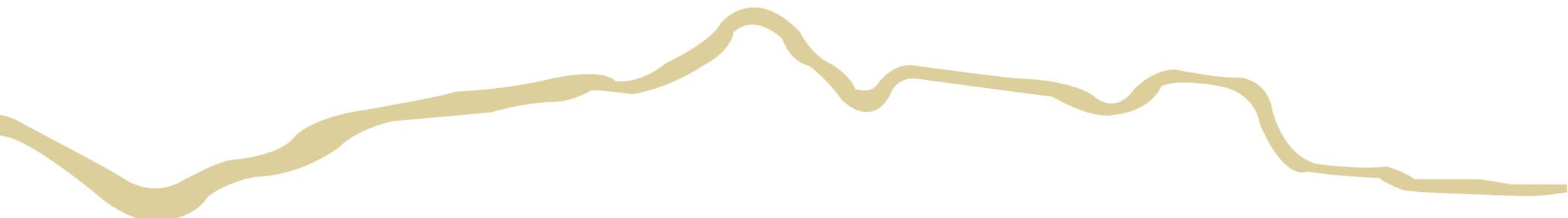




PROJECT ENTRIES

2.12

Greenway Signage Program



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	2.12 b.1	Logo Color Standards	2.12-3
	2.12 c	Information Trailhead Kiosks	2.12-4
	2.12 c.1	Materials Standards	2.12-4
	2.12 c.2	Information Standards	2.12-5
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	2.12 d.3	Locations	2.12-9
	2.12 d.4	Two-Column Monument	2.12-10
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	2.12 e	Secondary Gateway Monuments	2.12-14
	2.12 e.1	Materials Standards	2.12-14
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	2.12 e.3	Locations	2.12-15
	2.12 f	Mile Markers	2.12-16
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	2.12 g.1	Materials Standards	2.12-18
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	2.12 g.3	Locations	2.12-19
	2.12 g.4	Banners along Trails	2.12-20
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DEV. PLAN SYMBOL	SECTION	CONTENTS	PAGE
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	2.12 h.2	Signage Standards	2.12-22
	2.12 h.3	Mounting Systems	2.12-23
	2.12 h.4	Locations and Information	2.12-23
	2.12 i	Major Directional Signage	2.12-24
	2.12 i.1	Materials Standards	2.12-24
	2.12 i.2	Information Standards	2.12-24
	2.12 i.3	Signage Standards	2.12-25
	2.12 i.4	Locations	2.12-25
N/A	2.12 j	Minor Directional Signage	2.12-26
	2.12 j.1	Materials Standards	2.12-26
	2.12 j.2	Signage Standards	2.12-27
	2.12 j.3	Information Standards	2.12-27
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N/A	2.12 k	Roadway Approach Signs	2.12-28
	2.12 k.1	Materials Standards	2.12-28
	2.12 k.2	Signage Standards	2.12-29
	2.12 k.3	Median Approach Sign Standards	2.12-29
	2.12 k.4	Curbside Approach Sign Standards	2.12-29
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	2.12 l.1	Information Standards	2.12-30
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N/A	2.12 m	Trailhead Parking Signs	2.12-32
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	2.12 m.2	Signage Standards	2.12-33
	2.12 m.3	Parking Entry Sign Standards	2.12-33
	2.12 m.4	Parking Space Sign Standards	2.12-33

2.12 GREENWAY SIGNAGE PROGRAM

Effective signage aids navigation and is an important tool for educating users about trail rules, designated uses, and natural resources. A system of improved wayfinding that is easy to understand and follow will help to guide visitor traffic around the Greenway and enable visitors to plan routes and navigate them successfully. An improved signage system would enable trail users to clearly identify their location and planned destination, as well as communicate this information to other users, park staff, and emergency responders.

The design theme for the signage program developed in this document is inspired by the American Craftsman style, or the American Arts and Crafts Movement from the early 20th century. The featured signs, structures, chosen materials and associated graphics are intended as modern interpretations of this movement, and will be referred to in this document as a 'Contemporary Craftsman' style.

Signage Program Intent

- Highlight the Arroyo Simi Greenway
- Identify and unify the trail as a unique place
- Provide functional safety and traffic information
- Be uniform throughout the Greenway to create a consistent design character
- Include the Greenway logo as a unifying design element
- Be provided in a simplified format with easy-to-read fonts and concise text
- Incorporate simplified graphics that are easily understood
- Rely on universal graphic symbols for commonly used facilities along the trail
- Evoke a natural and timeless design aesthetic
- Whenever feasible, incorporate recycled materials

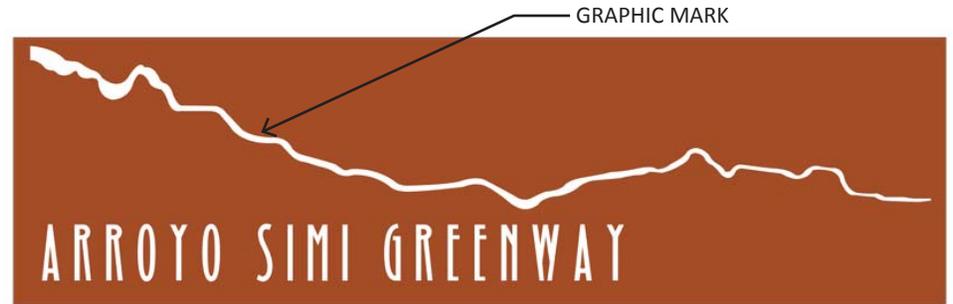
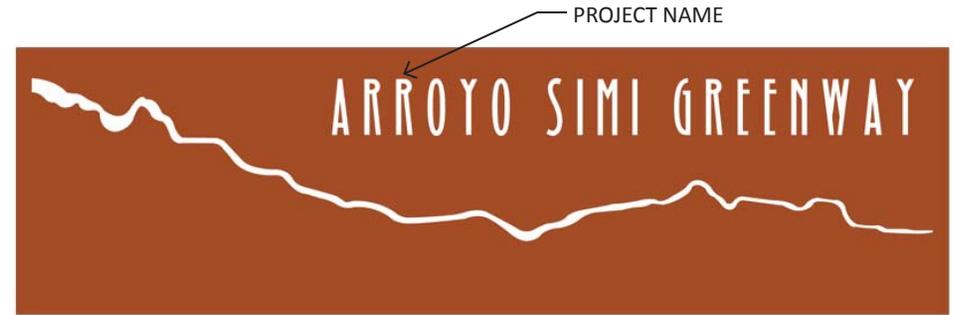
a. Greenway Logo

The Arroyo Simi Greenway Logo acts as an image-branding tool, which clearly and concisely identifies the Greenway as a unique and independent recreational experience within the City of Simi Valley.

The design of the Arroyo Simi Greenway Logo is inspired by the course of the Arroyo itself. The Arroyo's course, when seen on a map or viewed from the air, follows an interesting path that lends itself to becoming a very unique graphic mark. In combination with the project name, it is that uniqueness and simplicity that makes for a successful logo and branding of the Greenway. The Craftsman-inspired font for the logo is called "Binner Gothic" (TrueType) and it was chosen to compliment the design theme of the entire Greenway. Once sign fabrication is underway, font licenses will need to be purchased by the City in order to obtain legal use of the font for the signage program.

a.1 Logo Standards

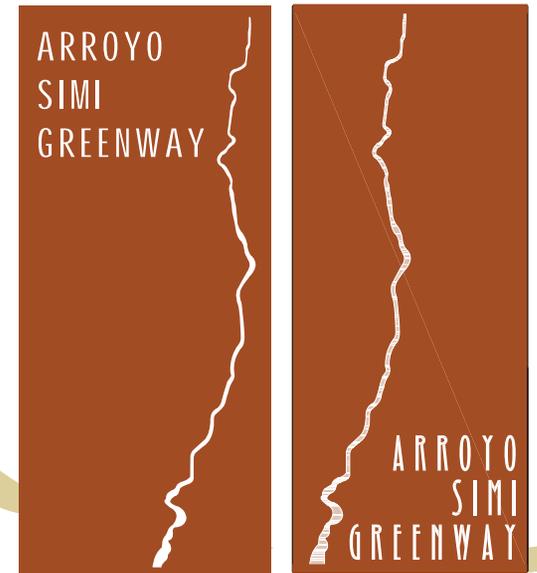
- The logo shall be legible at any size scale (from letterhead to large sign)
- The logo should lend itself to a variety of configurations and materials
- The logo shall read in black and white or the colors noted in Section 2.12b



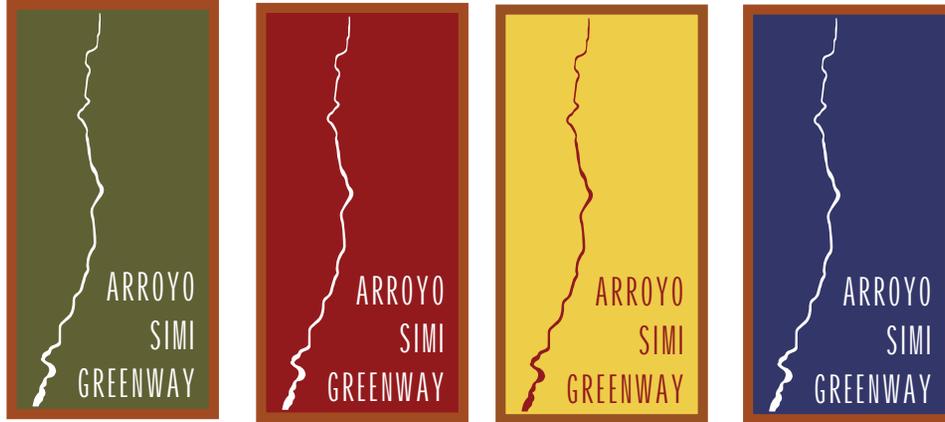
Horizontal Logo Options

The Arroyo Simi Greenway logo should consist of a combination of two pieces:

1. The graphic mark
2. The project name



Vertical Logo Options



b.1 Logo Color Standards

The following colors and their CMYK values (Cyan, Magenta, Yellow, and black pigments used for commercial and most color printers) should be used for the standard color logo palette:

- Green - 57, 46, 87, 31
- Red - 26, 100, 100, 27
- Yellow - 8, 16, 84, 0
- Blue - 92, 87, 25, 11
- Brown - 25, 78, 100, 19

b. Standard Logo Colors

Some of the signage along the Greenway is designed to feature the logo as a steel or aluminum cutout. There are, however, scenarios where color is applicable and/or preferred. In these cases, the Greenway logo should typically appear in white, excepting the red logo when used with yellow, and the Logo Color Standards applies for the background.

Appropriate uses of the logo in color include, but are not limited to:

- Greenway Banners
- Signage that cannot accommodate a steel cut-out construction



c. Information Trailhead Kiosks

Informational Kiosks provide a focal element marking the trailhead and directing trail users to the many destinations along the Greenway from their current trailhead location. Kiosks shall integrate the Greenway design theme, and act as an icon visible from public parking associated with the Greenway.



Information Kiosk

c.1 Materials Standards

- Galvanized Corrugated Iron, stainless steel, or recycled steel roofing
- Recycled Steel posts and frame, painted powder coated to simulate weathered steel or corten steel
- Recycled Aluminum Logo, laser cut and mounted to recycled steel backing plate on kiosk
- Fiberglass Embedded or High Pressure Laminate graphic panels for maps and associated informational signage



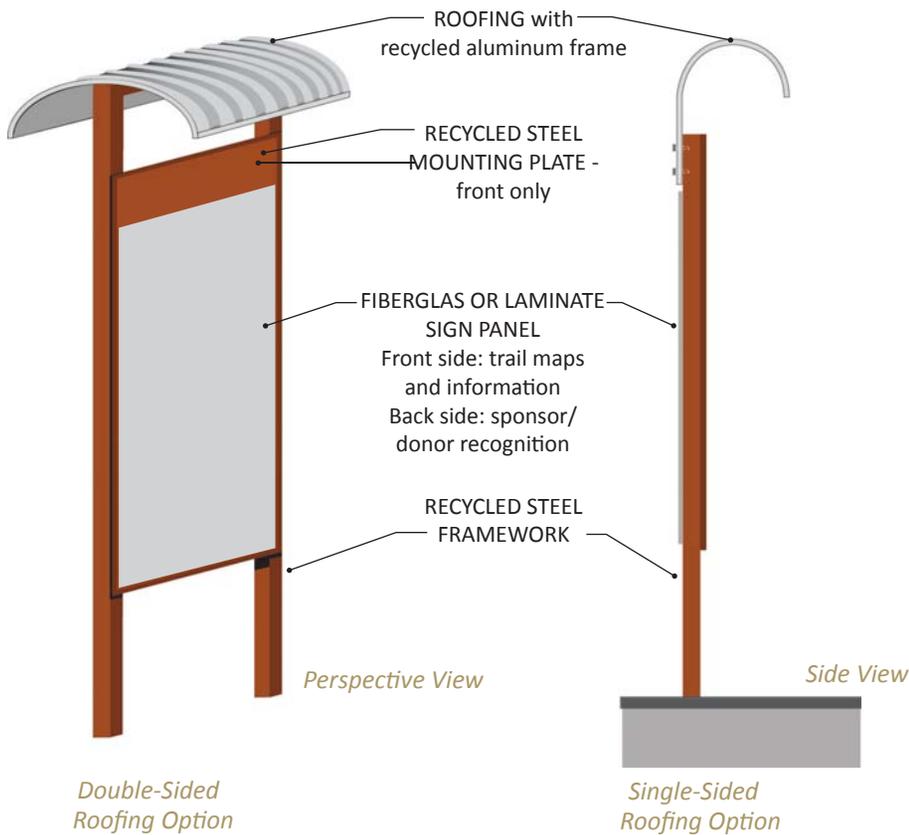
c.2 Information Standards

All Informational kiosks shall include the following information:

- Arroyo Simi Greenway Logo
- Trailhead name (such as “Rancho Santa Susana Park Trailhead”)
- Greenway Trail System Map (see Major Directional Signage, Section 2.12i) with mile markers (Section 2.12 f)
- Information on the Kiosks should be coordinated with Minor Directional Signage – Location Identification (Section 2.12 j)
- Distances to other trailheads along the Greenway
- Rules of the trail and trail hours of use
- City and District Logos
- Regional trail connections to other adjacent trail systems (such as trails in Corriganville Park or Mt McCoy Trail)
- Logos of contributing organizations and/or sponsors
- Trail navigation information and/or map from trailhead to the Greenway, where necessary
- Optional notice boards or advertising for sponsors on the back side of the kiosk

c.3 Locations

- Tierra Rejada Trailhead
- Rancho Santa Susana Community Park
- Rancho Simi Community Park
- Darrah Volunteer Park
- Sycamore Drive Community Center
- Corriganville Park
- West of Vista Del Arroyo Park



Double-Sided Roofing Option

Single-Sided Roofing Option



Informational Kiosk Locations



INFORMATION KIOSK



*Sycamore Drive Community Center Trailhead
plan view*

c.4 Signage Standards



INFORMATION KIOSK



Sycamore Drive Community Center Trailhead
view from Sycamore Drive

- The Informational kiosks should be easily seen from the parking area, but not located to interfere with critical sightlines
- Kiosks roofing may be double-sided or single-sided depending on location and solar aspects
- Provide 5 feet clear minimum around all sides of the kiosk to allow for trail users to view the sign and not interfere with the flow of traffic
- Informational kiosks shall be located so that the front faces the side with the most anticipated use and visibility
- Minor Directional Signs and Greenway Banners should be used when necessary to guide trail users to the Information Kiosk, Trailhead Parking and/or Greenway trail system
- Informational kiosks shall be located near trailhead/parking areas along the main path to the Greenway



d. Primary Gateway Monuments

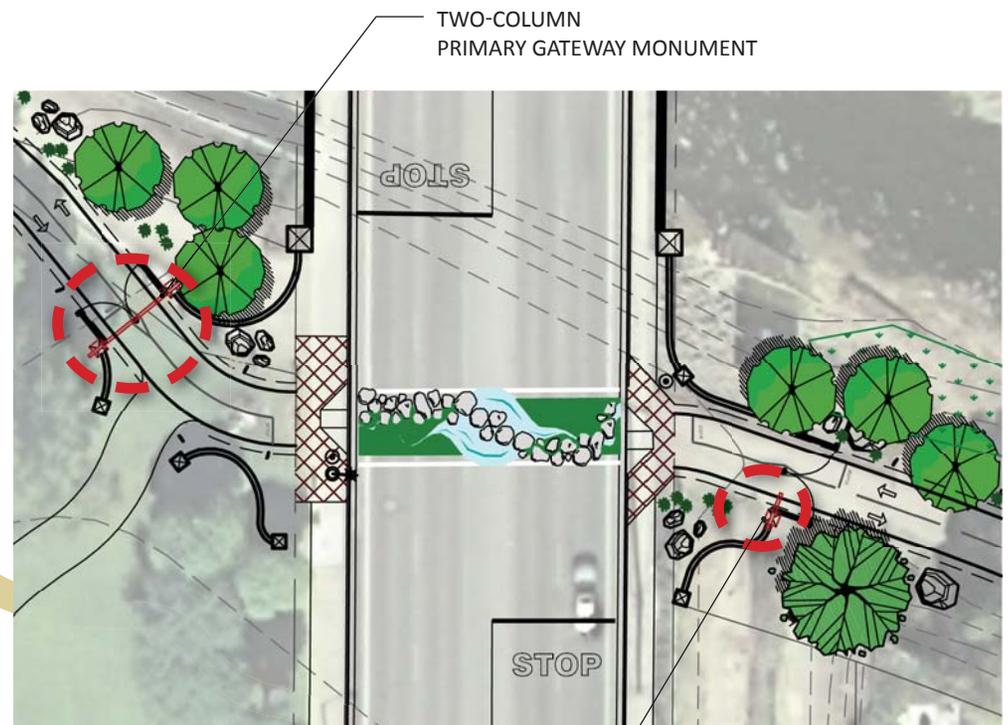
The Primary Gateway Monuments will announce the Greenway to users and passing traffic at all Primary Gateway Locations. It serves to clearly mark the Greenway and creates an entry experience for trail users. The identified locations for this feature also provide ample exposure for the Greenway to residents and visitors alike, creating an icon that identifies the Greenway as a significant feature of Simi Valley and becomes a part of the fabric of the City. Primary Gateway Monuments are part of a Primary Gateway Entry scene (refer to Section 2.11 for additional information).

d.1 Materials Standards

- Masonry base (refer to Masonry Standards 2.12-13)
- Recycled corten steel posts and beams
- Recycled corten sign panel, with logo cut out
- Optional lighting feature (see Steel Sign Box Detail on page 2.12-10) if lighting feature is used, provide conduit and pull box in post and beam

There are two types of Primary Gateway Monuments that are used together to emphasize the Greenway:

1. **Two-Column Monument**, generally placed on the side of the cross street with the most anticipated use and visibility, on trails identified as the Primary trail alignment on the Site Development Plan (refer to Section 2.1).
2. **One-Column Monument**, created to reduce capital improvement costs and fit in spaces lacking room for the Two-Column Monument, placed on the opposite side of the cross street from the Two-Column Monument.



ONE-COLUMN
PRIMARY GATEWAY MONUMENT

d.2 Signage Standards

- The monuments shall have a minimum vertical clearance of 14 feet when located over the trail
- The gateway monuments shall be easily seen from the roadway, but not interfere with critical sightlines (refer to following pages for dimensions)
- The same materials, color scheme, and construction methods shall be used on all Primary Gateway Monuments to unify the Greenway, and shall incorporate the Arroyo Simi Greenway logo (Refer to Section 2.12a)
- Monuments shall be positioned perpendicular to the trail
- Incorporate Primary Gateway Monuments with walls and fencing wherever feasible and space allows
- Primary Gateway Monument signs shall be double sided, and the logo shall be legible when entering and exiting the trail



Sycamore Drive Community Center Trailhead
view from Sycamore Drive

d. PRIMARY GATEWAY MONUMENTS



Primary Gateway Locations

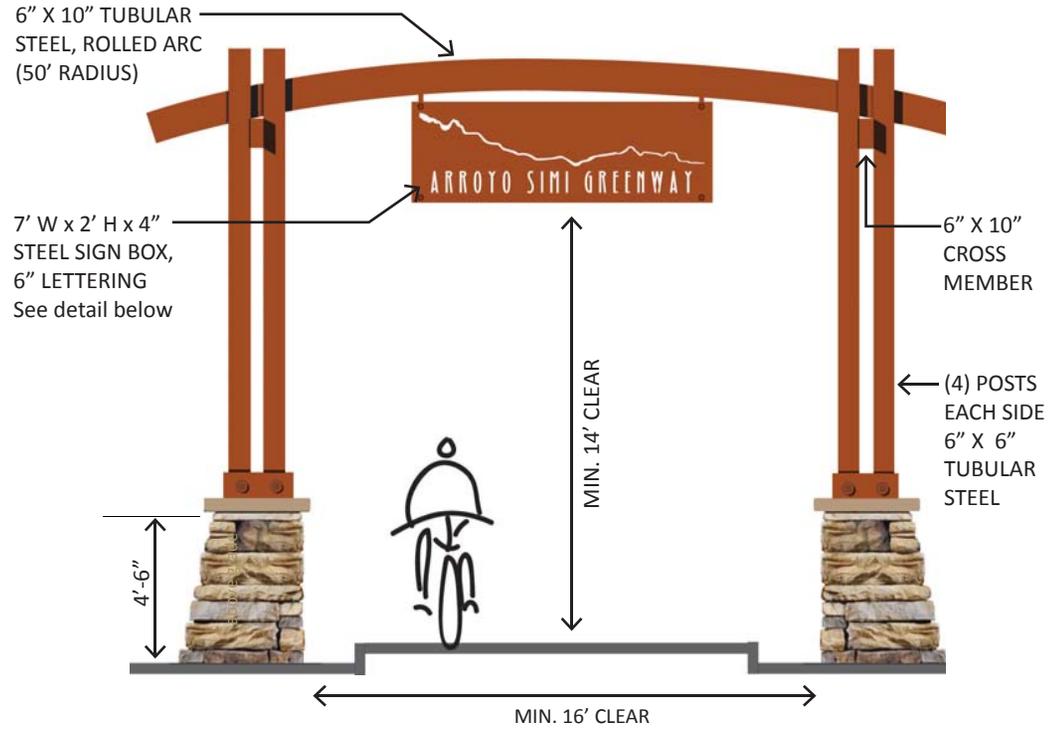
d.3 Locations

- Madera Road
 - Two-column, east side
 - One-column, west side
- Erringer Road
 - Two-column, west side
 - One-column, east side
- Sycamore Community Center Trailhead
 - Two-column only (not an intersection)
- Sequoia Avenue
 - Two-column, west side
 - One-column, east side
- S-Curve Potential Future Park at Bike Roundabout
 - Two-column only (not an intersection)
- Kuehner Drive
 - Two-column, west side
 - One-column, east side

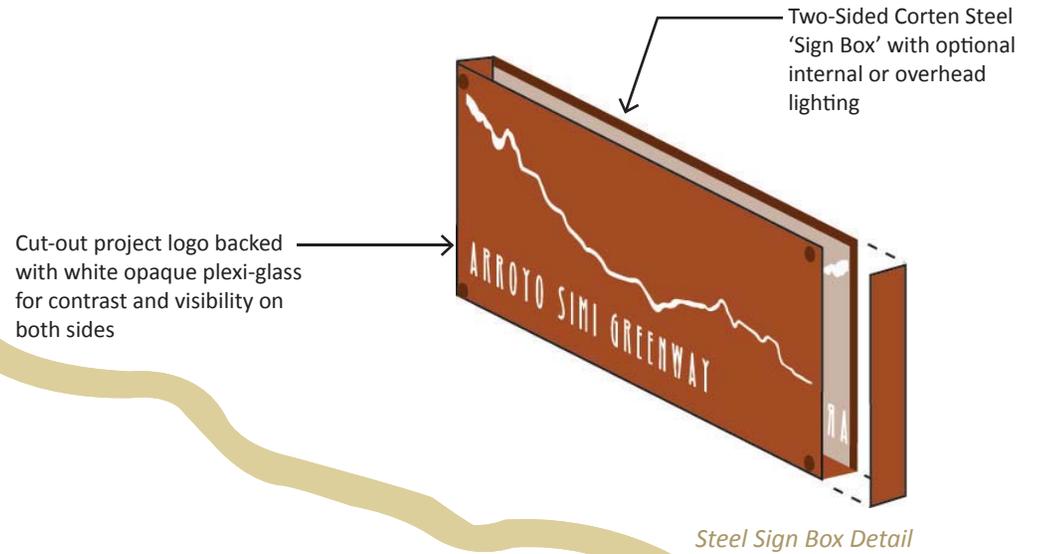


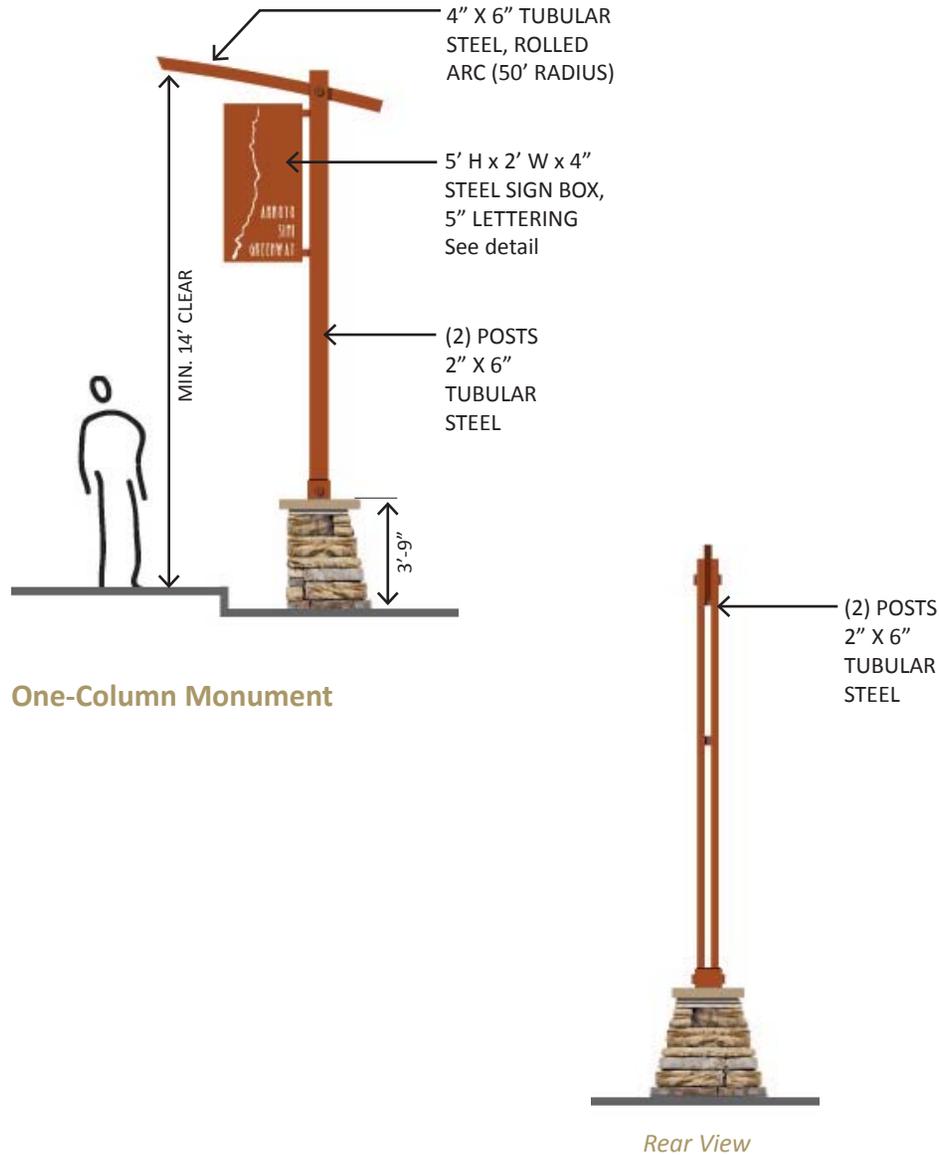
d.4 Two-Column Monument

The Two-Column Monument consists of two stone-veneered bases, with four vertical steel posts rising out of each base. At the top the post rests an overhead arching beam, from which Greenway signage will hang. This whole structure straddles the Greenway, allowing space for the trail and clearance for maintenance vehicles.



Two-Column Monument





d.5 One-Column Monument

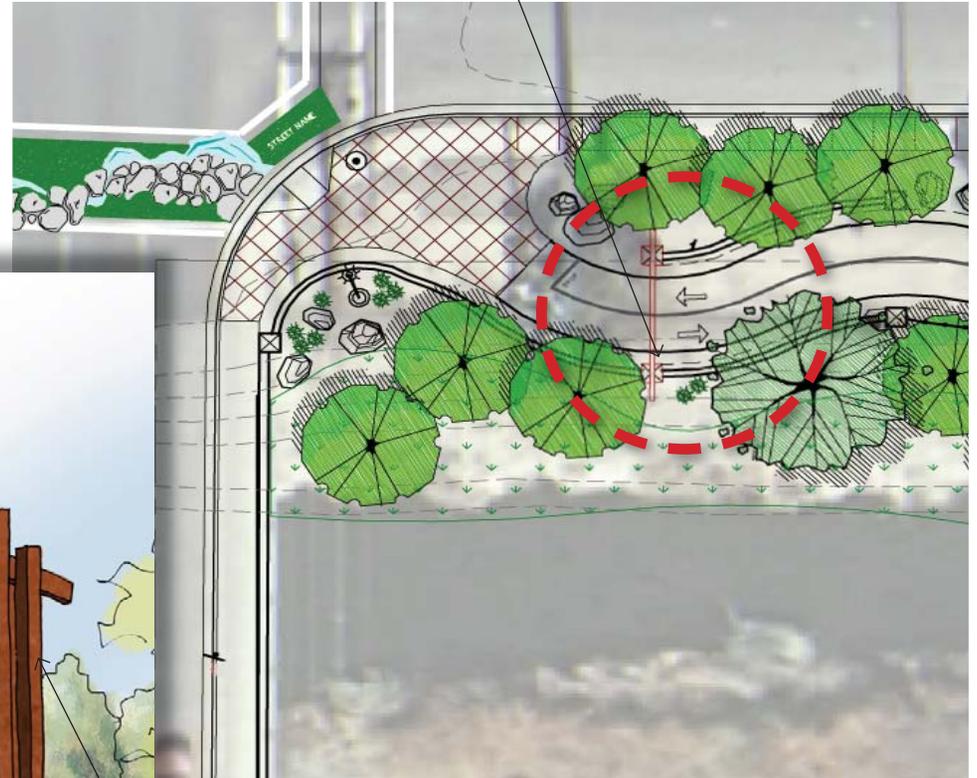
The One-Column Monument is designed as a single stone-veneered base, atop which sits two steel vertical posts. From those vertical posts, an additional arching steel beam is cantilevered out over the Greenway. One-Column Monuments should be placed on the side of approaching users entering the trail, with the sign box located perpendicular to the trail and the column and sign clear of the paved trail.



d. PRIMARY GATEWAY MONUMENTS



Madera Road Primary Gateway
view from Madera Road



TWO- COLUMN
PRIMARY GATEWAY
MONUMENT

Madera Road Primary Gateway
plan view

TWO- COLUMN
PRIMARY GATEWAY
MONUMENT

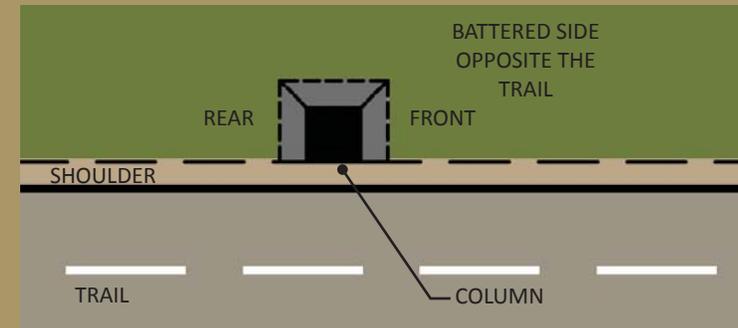
d.6 Masonry Signage Standards

Greenway masonry should be constructed to reflect the design aesthetic set forth in this Signage Program and in the interest of durability and maintenance concerns, no manufactured or artificial stone material should be permitted:

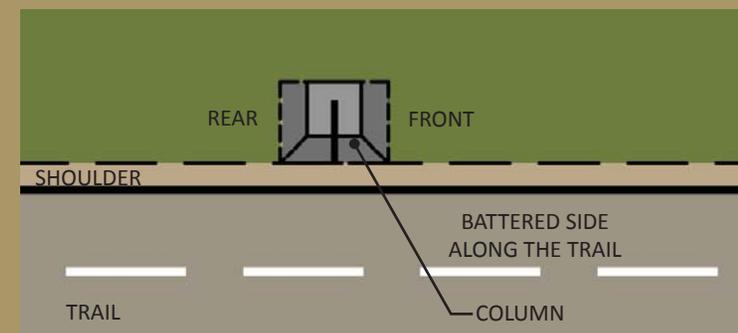
- **Stone:** All material shall consist of natural stone with minimum veneer depth no less than 3 inches thick. Selected stone shall consist of moderately sized rectangular pieces.
- **Stone Pattern:** All stone veneer shall be constructed with a course ashlar pattern or river rock. The stone veneer pattern should include variation in course heights, and may incorporate occasional broken bond courses. All exposed corners shall be constructed such that the length of the stones alternate on each side of the corner, creating a continuous dovetailed effect.
- **Stone Color:** Stone color shall range from medium-light to medium-dark cream, brown or beige tones.
- **Grout :** Grout shall be natural color and shall be finished with a concave, "v" shaped or rooded joint.
- **Post Caps:** Cap material may consist of either natural cut stone or precast concrete with integral color. Cap stone color should be compatible and blend with the selected stone veneers colors. Cap edges should be eased or include rounded edges to minimize likelihood of chipping.
- **Anti-Graffiti Coating:** All surfaces of finished stone veneer, grout and post caps shall receive two (2) coats of approved low-sheen anti-graffiti coating

➤ Number of battered sides:

- Primary Gateway Monument (One and Two-Column)
 - 3 battered sides
 - Front, rear and side opposite of the trail



- Secondary Gateway Monument and Major Directional Signage
 - 3 battered sides
 - Front, rear and side along the trail

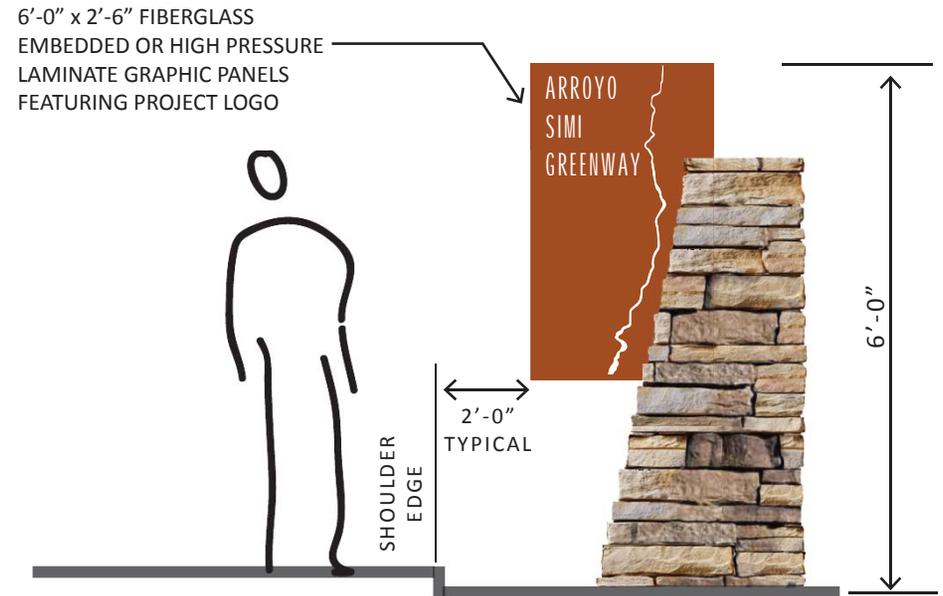




e. Secondary Gateway Monuments

Secondary Gateway Monuments identify less prominent Greenway entry points. These featured structures are intended to clearly mark the arrival into the Greenway, but on a smaller scale than the Primary Gateway Monuments. Secondary Gateway Monuments are part of a Secondary Gateway Entry scene; refer to Section 2.11 for additional information.

e. SECONDARY GATEWAY MONUMENTS



Secondary Gateway Monument

e.1 Material Standards

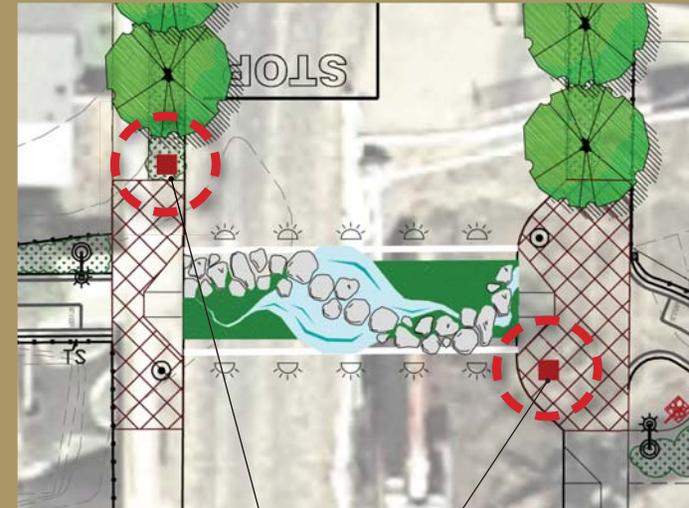
- Masonry base (refer to *Masonry Standards, 2.12d*)
- Sign is a fiberglass embedded or high pressure laminate graphic panel

e.2 Signage Standards

- Monuments shall be positioned to be visible from adjacent roadways, but not interfere with critical sightlines
- The same materials and color scheme shall be used on all Secondary Gateway Monuments to unify the Greenway
- Sign background shall be brown as described in Section 2.12b, and additional lettering and Greenway logo shall be sharply contrasting bright white
- All directional sign lettering shall match Greenway logo described in Section 2.12a, and be a minimum of 3 inches high
- Signage corners shall be rounded
- Incorporate an Arroyo Simi Greenway logo on both sides of the monument (Refer to Section 2.12a), and ensure that it is visible and legible when entering and exiting the trail
- Monuments shall be located on the side that trail users are entering the Greenway, positioned perpendicular to the trail, and placed along the Primary trail alignment identified in the Site Development Plan, (refer to Section 2.1)
- Incorporate Secondary Gateway Monuments with walls and fencing whenever feasible

Yosemite Avenue Secondary Gateway

plan view



SECONDARY GATEWAY MONUMENT



e. SECONDARY GATEWAY MONUMENTS

Secondary Gateway Locations



e.3 Locations

- Moorpark and Simi Valley City Limits
- Waste Water Treatment Easement at LA Avenue
- Easy Street Potential Future Park
- Los Angeles Avenue
- First Street
- Fifth Street
- Sycamore Drive Bridge
- Existing Drainage Trail by Community Center
- Darrah Volunteer Park
- Tapo Street
- Tapo Canyon Road
- Royal Avenue
- Stearns Street
- Stow Street
- Yosemite Avenue
- Katherine Road

f. Mile Markers

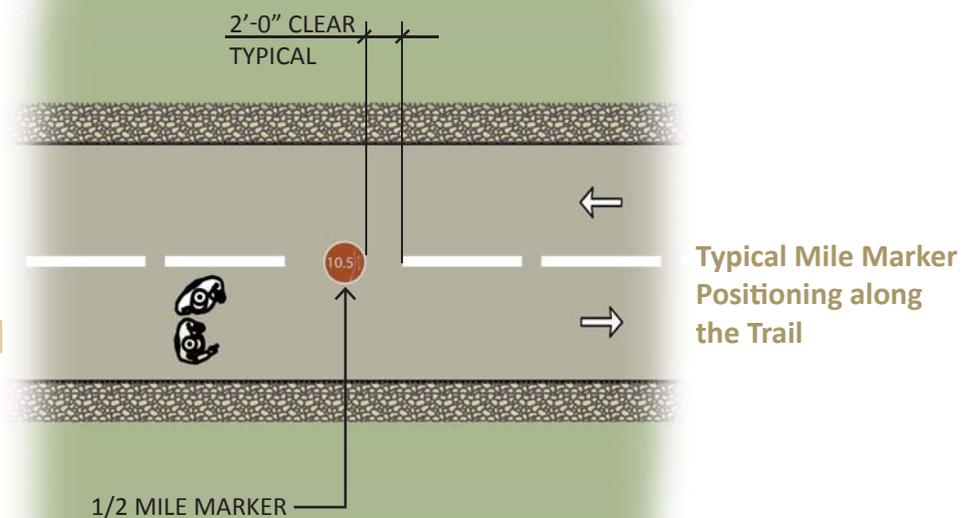
Mile Markers shall be placed along the Greenway to alert users as to their location within the Greenway, facilitate maintenance and security operations, as well as provide indicators as to how far trail users have traveled. Pedestrians and bicyclists will have a sense of comfort and familiarity in identifying their location along the trail. Repeating forms and elements provides users with a sense of place and location, while unifying the trail system.

f.1 Materials Standards

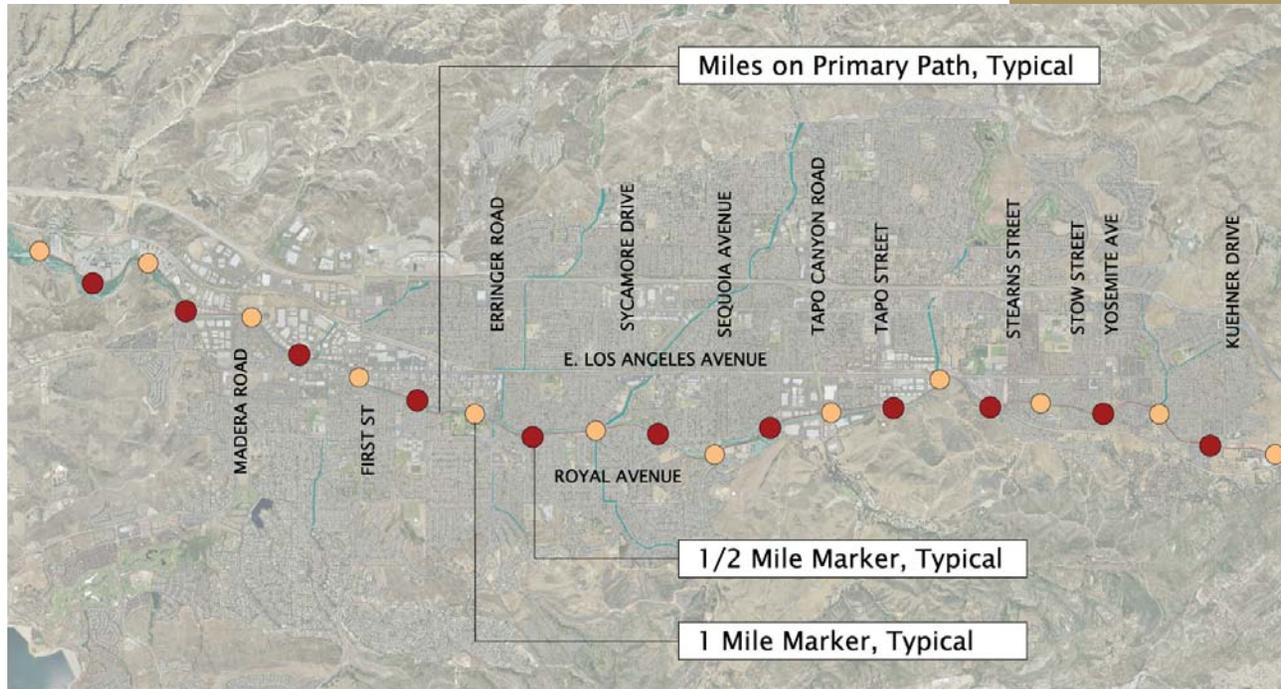
- Mile Markers shall be constructed of Thermal Plastic Paint, or sustainable alternative

f.2 Signage Standards

- Mile Markers shall contain ½-mile and 1-mile text and a Arroyo Simi Greenway graphic mark
- Mile Markers shall be 2 feet in diameter and located along the center line of the trail and be visible to trail users traveling in either direction
- ½-mile markers shall be gray with a brown border and 7-inch text, and the 1-mile markers should be brown with a grey border and 7-inch text
- 2 feet clear shall be maintained around the Mile Markers to allow for visual impact and potential future maintenance
- Mile Markers shall be oriented with the top to the north side of the trail
- Mile markers shall be shown on all Major Directional Signage maps (*refer to 2.12i*) and Informational Kiosk maps (*see Section 2.12 c*)

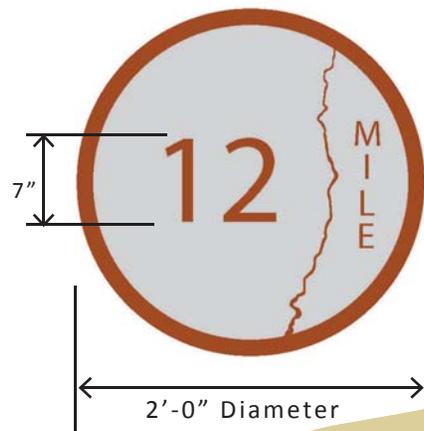


Typical Mile Marker Positioning along the Trail

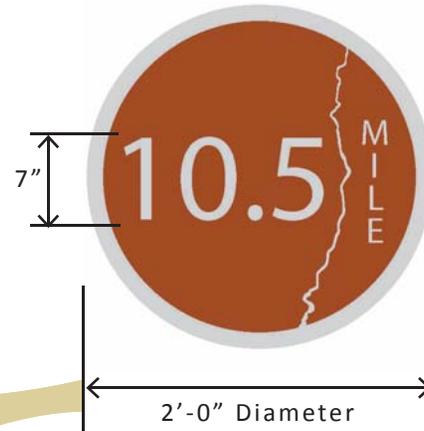


f. MILE MARKERS

Mile Trail Markers



Half-Mile Trail Markers



f.3 Locations

- At ½-mile and 1-mile intervals
- Mile Markers should be located along the Primary trail alignment as identified in the Site Development Plan in Section 2.1, but not spurs and access trails, and should be measured along the trail centerline (not by concentric circles)
- Begin mileage at Corriganville Park (Mile 0), so the first marker is ½ mile away from the start of the trail at Corriganville Park

g. Greenway Banners

There are two primary types of uses for the Greenway Banners:

1. **Banners along streets**, used to publicize the Greenway, direct trail users to the Greenway, and highlight trailhead parking areas
2. **Banners along trails**, used to add color and vertical elements in spaces that are too small and restricted for landscaping

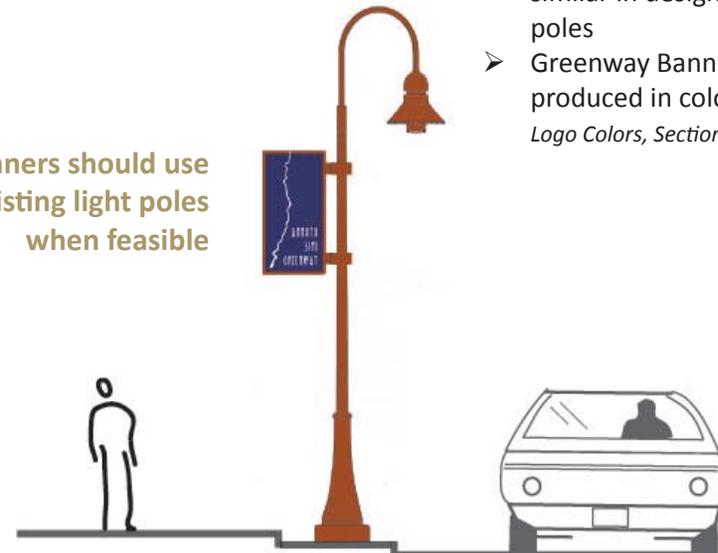
g.1 Materials Standards

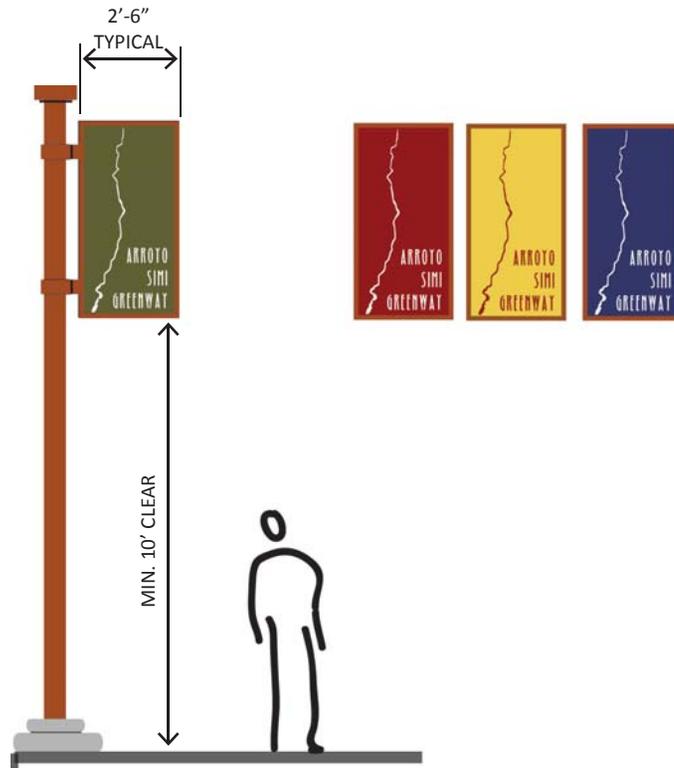
- Banners shall be made from outdoor quality vinyl mesh, in a glossy finish
- Banners supports shall have break-away arms
- Recycled aluminum or steel poles and frames, painted to look like weathered steel

g.2 Signage Standards

- The lowest point of the banner must be a minimum of 10 feet above the trail, sidewalk or roadway, whichever is highest
- Banner lettering shall be clear, precise, and simple with minimal graphics to avoid distracting trail users and creating safety hazards
- Lettering shall be 5 inches high minimum and with at least 1 inch clear from banner edge
- Banner lettering and graphic mark shall match the Greenway logo described in Section 2.12a
- Banner graphic mark shall run the entire width of the banner, with at least 1 inch clear from lettering and 2 inches clear from the lower left corner
- The Greenway Banners shall contain graphics on both sides, and shall be legible in either travel direction
- Banners shall be located perpendicular to approaching vehicular and trail user traffic
- Greenway Banners adjacent to private property shall not block neighboring views or existing access
- Banners shall be located on existing or proposed light poles wherever feasible, and banners poles shall be similar in design to proposed light poles
- Greenway Banners shall be produced in color (*refer to Standard Logo Colors, Section 2.12b*)

Banners should use existing light poles when feasible

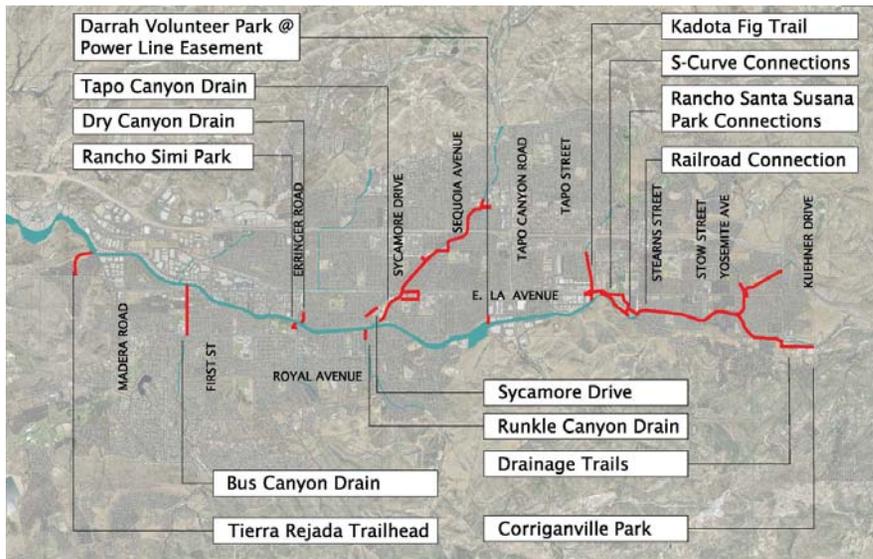




Greenway Banner Color Options

A banner color scheme could be used to indicate the type of use for which the banner is intended, and if it is used it shall be consistently used throughout the Greenway to aid in trail use and navigation.

- Use green banners along trails
- Use blue banners along existing and proposed sidewalk trails
- Use red banners between trailheads and the Greenway
- Use yellow banners at trailheads



Greenway Banner Locations

g.3 Locations

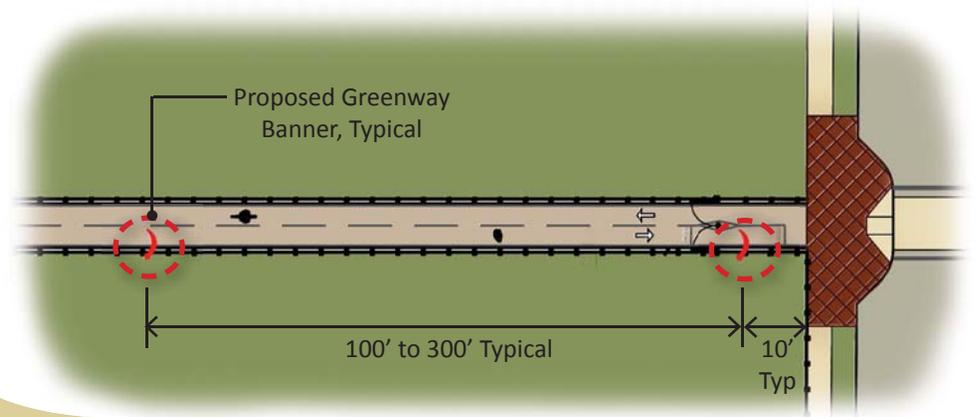
- Along Drainage and Power Line Easement Trails
- Along Existing and Proposed Sidewalk Trails
- Between Trailheads and the Greenway
- At Trailheads

Place banners on drainage side of the trail and face them away from trail



g.4 Banners along Trails

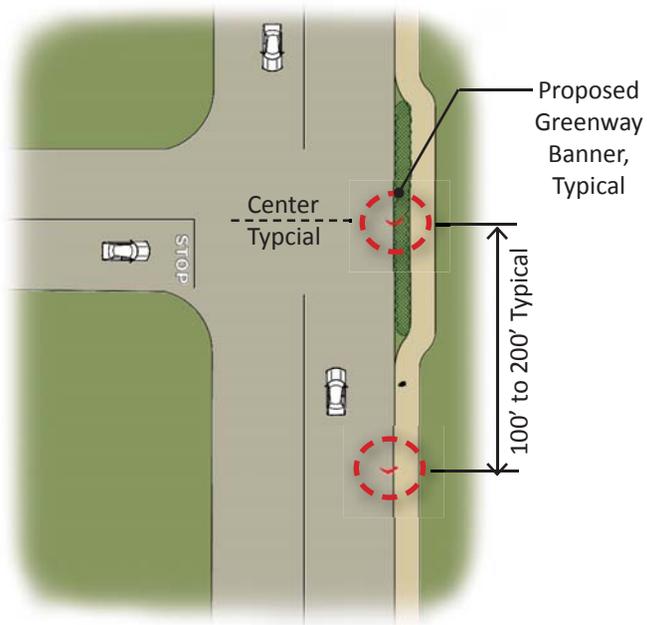
- Banners shall start 10 feet from back of sidewalks at entries
- Banners shall be placed approximately 100 to 300 feet on-center, and should be located 3 feet from the edge of trail when feasible
- Banners shall be placed along the drainage side of the trail (if applicable)
- Banners along trails shall face the banner away from the trail
- Banners shall be placed in the center of existing or proposed fence post locations
- Banners shall attach to drainage wall when there is not enough room for a base on the trail



Typical Banner Locations along Trails

g.5 Banners along Streets

- Banners shall be located approximately 100 to 200 feet on-center, and placed approximately 1 foot from the back of curb or centered in parkways (if applicable)
- Banners shall not be located on private property
- Banners along streets shall face the banner towards the sidewalk, and away from the roadway



Typical Banner Locations along Streets



h. Interpretive Exhibits

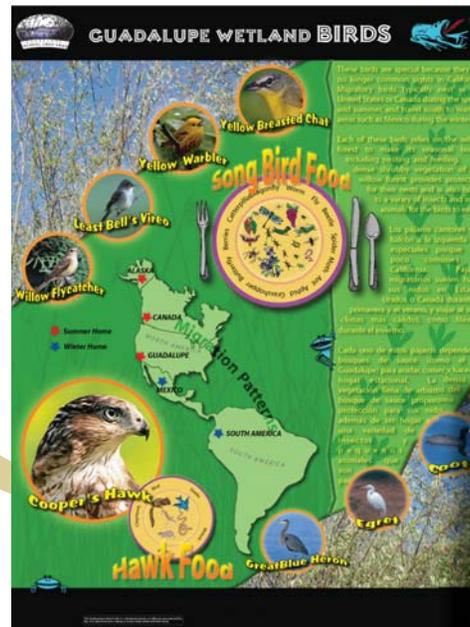
The Greenway Interpretive Exhibits offer a unique opportunity for education in a variety of subjects, and should be placed at strategic locations along the trail. Exposing trail users to informative and interesting interpretive signage fosters a deeper sense of connection with the local community and the surrounding natural environment, a better understanding of natural and historic processes and their effects, and enthusiasm about the health benefits of using the trail itself. The interpretive signage should include information regarding the sustainable features and programs along the Greenway.

h.1 Materials Standards

- Greenway Interpretive Exhibit panels shall be fabricated from Fiberglass Embedded or High Pressure Laminate graphic panels
- Bases, pedestals/mounting systems shall be mounted on metal or steel posts that are weather resistant and low-maintenance, and may be freestanding or mounted onto the railing

h.2 Signage Standards

- All Interpretive Signs shall incorporate a design theme and aesthetic that fits the Greenway and incorporates the individuality of the particular site and panel topic
- Interpretive Signs shall not exceed 16 square feet in area, and can vary in size to fit the individuality and character of the site
- A hierarchy of fonts which includes the Greenway Logo font used on signage shall be used on every interpretive sign
- Interpretive Signs should contain Spanish translations
- All Interpretive Signs shall incorporate an Arroyo Simi Greenway logo
- Interpretive Signs may consist of photographs or illustrations
- A clean and concise graphic layout which complements the other elements within the Greenway Signage Program shall be used
- Orient interpretive signage to limit surface area exposed to direct sunlight
- Locate interpretive signage and/or exhibits with a minimum of 5 feet of clearance so they will not conflict with pedestrian or bicycle circulation
- Carefully locate and limit interpretive signage and/or exhibits so as not to clutter the viewshed, rest area/overlook or trail



Interpretive Exhibit Examples

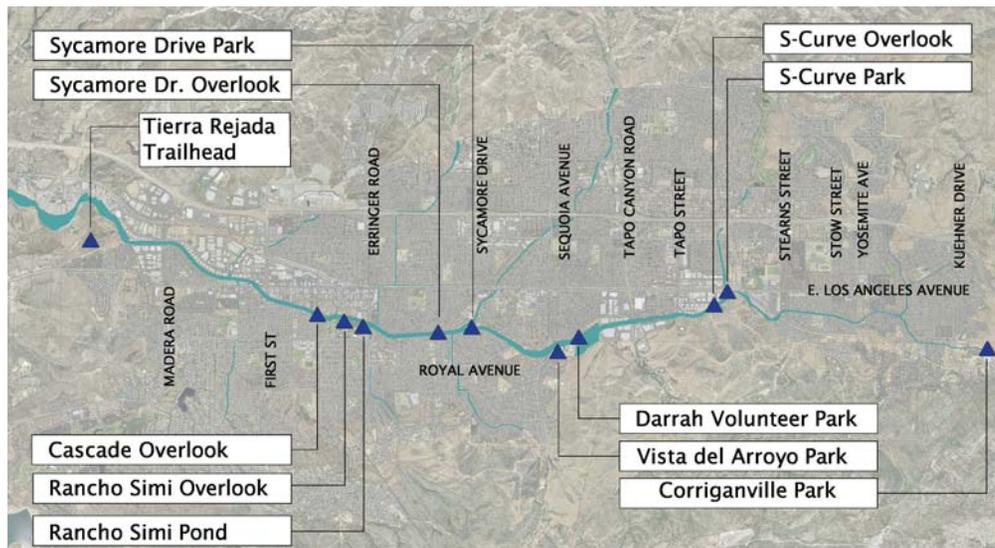
h.3 Mounting Systems

Mounting Systems for the interpretive signage could include, but are not limited to the following options:

- Standard steel pedestals, provided by fabricator
- Custom-Designed pedestals/mounting systems, in kind with the established materials and design theme of the Greenway signage and/or stone walls or fencing
- Custom-Designed pedestals/mounting systems may include public art (refer to *Focal Points/Public Art Opportunities, Section 2.15*)



Examples of Interpretive Exhibit Mounting Systems



Interpretive Exhibit Locations

h.4 Locations and Information

The following list contains potential locations for Interpretive Exhibits, and correlating ideas for sign content:

- Tierra Rejada Trailhead
 - Historical and cultural aspects of native plants
- Cascade Overlook west of Rancho Simi Community Park
 - The watershed and water quality
- Rancho Simi Community Park Overlook
 - The history of Simi Valley
- Rancho Simi Community Park Pond
 - Wildlife found in the Arroyo Simi
- Sycamore Drive Overlook
 - Air quality improvements through bicycle commuting
- Sycamore Drive Triangle Potential Future Park
 - The importance of riparian habitats
- Vista Del Arroyo Park
 - History & importance of Arroyo Simi as primary drainage & flood control/Role of VCWPD
- Darrah Volunteer Park Potential Future Expansion
 - Plant communities and ecology
- S-Curve Overlook
 - Geology of Simi Valley and the Arroyo Simi
- S-Curve Potential Future Park
 - Airport Development in Simi Valley
- Corriganville Park
 - Corriganville history

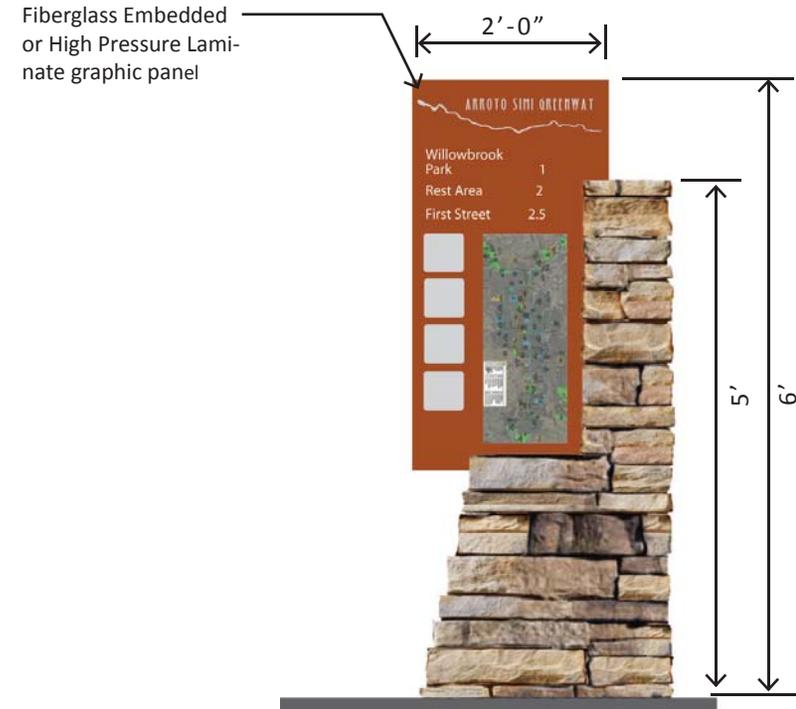
i. Major Directional Signage

Directional signs indicate specific destinations within the Greenway. They provide clarification and enhance the sense of distance and overall connectivity within the 12-mile Greenway.

Major Directional Signage is envisioned to be fiberglass panels similar to those used for Interpretive Exhibits (*Section 2.12h*). They can be designed with as a standard template with maps and layout similar for each location, but text and “You are Here” varying based on signage placement. The templates should be saved at the sign manufacturers, and easily reproducible if additional signs are deemed necessary.

i.1 Materials Standards

- Stone veneer in a coursed ashlar pattern (*see Masonry Signage Standards, Section 2.12d.6*)
- Recycled Steel Sign Backing
- Fiberglass Embedded or High Pressure Laminate graphic panels for maps and sign



Major Directional Signage

i.2 Information Standards

Major Directional map signage shall contain the following information:

- Greenway Trail System Map with mile markers shown along the Greenway to allow for additional navigation (*Section 2.12 f*) and National Parks symbols depicting uses such as picnic areas and restrooms at select key locations, and coordinated with Minor Directional Signage – Location Identification (*Section 2.12 j*) and Informational Kiosks (*Section 2.12 c*)
- Distances listed to major selected destinations along the Greenway and public uses within an approximate radius of 2 miles, plus one potential key feature further away
- Lettering on the directional map shall minimal with symbols used, rather than text, whenever feasible

Major Directional Sign



Major Directional Signage at Darrah Volunteer Park

i.3 Signage Standards

- Directional signs shall be prominently placed to increase visibility
- A clean and concise graphic layout which complements the other elements within the Greenway Signage Program shall be used, and the same materials and hierarchy of fonts shall be used throughout the Greenway
- Provide a minimum of 5 feet clear on front and sides of sign to allow for trail users to step off trail to view the sign
- Sign scale and height of information presented should be in proportion to trail users

- Major Directional Sign graphic panel background shall be brown as described in Section 2.12b, and additional lettering and Greenway logo shall be sharply contrasting such as a bright white
- If the trail is to be used at night, directional signs shall be well lit
- All directional sign lettering shall be a minimum of 1 inch high, excluding directional map lettering which can be a minimum of ½ inch high, and shall be clear, simple, and easy to read from a distance
- Signage corners should be rounded whenever feasible, and 2 inches clear should be maintained around sign panel perimeter



Major Directional Sign Locations

i.4 Locations

- City Limits Gateway
- Madera Road Rest Area
- Easy Street Potential Future Park
- Cascade Overlook
- Sycamore Drive Community Center Park
- Vista del Arroyo Park
- Darrah Volunteer Park Potential Future Expansion
- Royal Avenue Potential Future Park
- Tapo Canyon Road Overlook
- S-Curve Potential Future Park
- Yosemite Avenue Gateway
- Trailhead parking areas if Information kiosk is not provided (refer to Section 2.4)

j. Minor Directional Signage

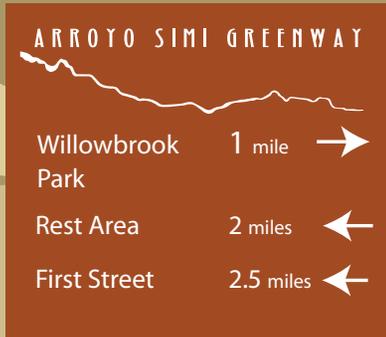
There are two primary types of uses for the Minor Directional Signs:

1. **Minor Directional Signs**, used to direct trail users to select primary destinations along the Greenway
2. **Minor Directional Signs - Location Identification**, used to identify reaching the select primary destinations along the Greenway

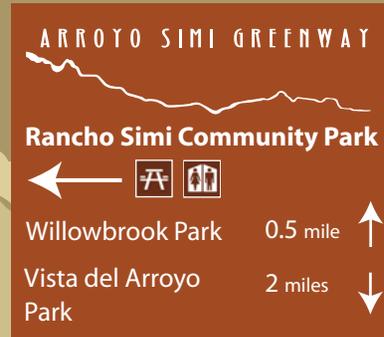
All Minor Directional Signs shall be used in conjunction with other trail signs to economize on the number of signs and overall cost. Minor Directional Sign will often be used with a Traffic and Safety Sign and/or Street Approach Sign (*refer to Section 2.12 I for additional information*). Depending on trail use, Traffic and Safety Signs could be located on top of or behind a Minor Directional sign.

A Street Approach Sign with the upcoming street name is intended to notify trail users that they are approaching a street intersection, as well as orient them to their location. In most cases, this sign will also include Traffic Safety Signage giving trail users appropriate warnings as they near the upcoming street.

Minor Directional Signage

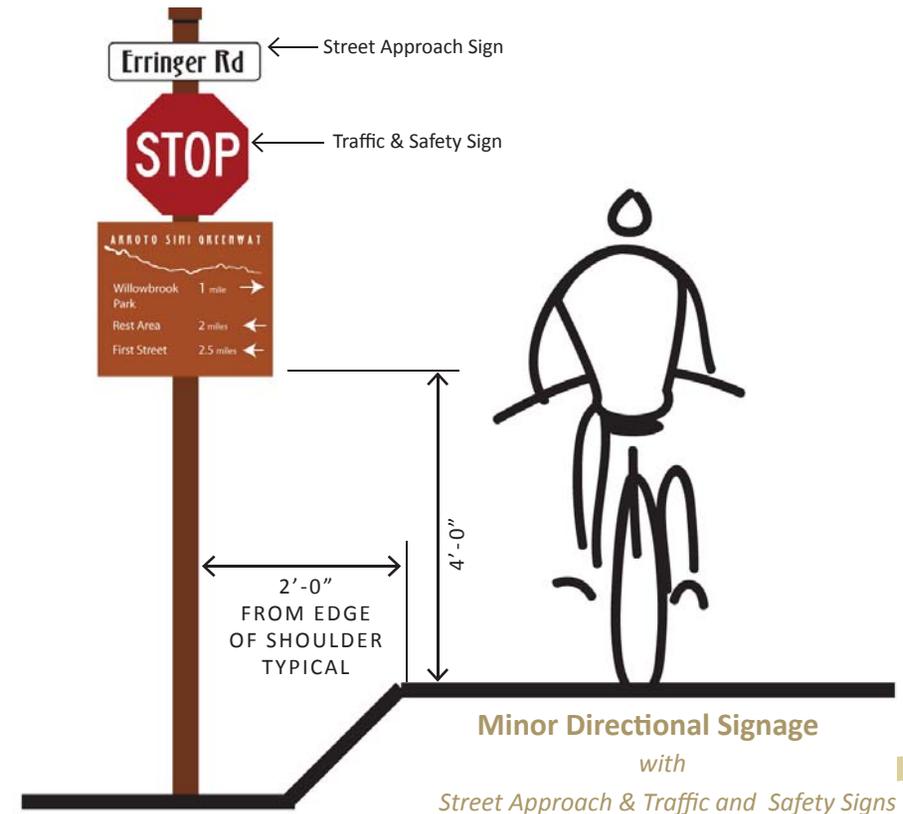


Minor Directional Signage
Location Identification



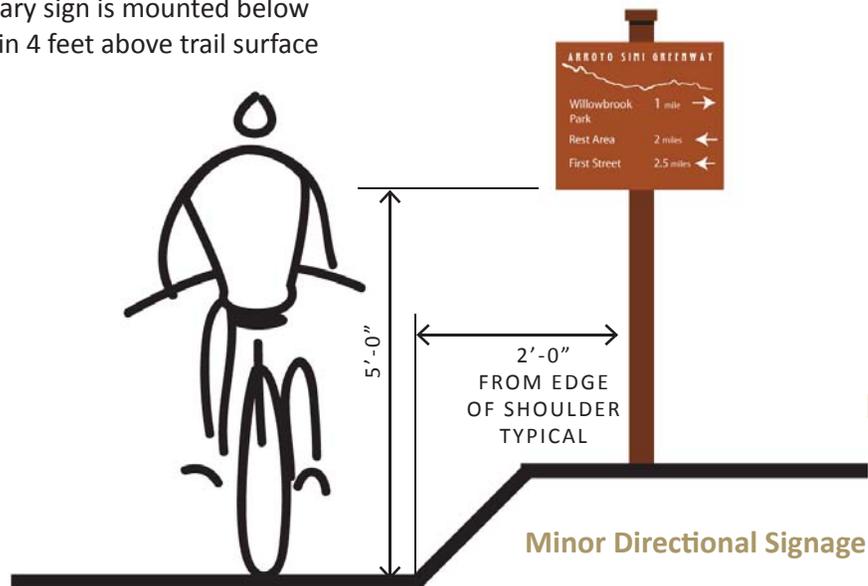
j.1 Materials Standards

- Metal sign panel backing with painted highly reflective white lettering/graphics
- Steel sign post, 4 to 6 inches square, to match Greenway signage



j.2 Signage Standards

- All Minor Directional Signs shall reflect the Greenway design aesthetic, while still adhering to the standards set forth in Section 2.12l of this Signage Program
- Traffic and Safety Signs shall share same pole as Minor Directional Sign when feasible
- Minor Directional Signs shall add a Street Approach sign to the top of the post at all road crossings, and it shall be double sided and read when approaching and exiting the Greenway
- All Minor Directional Signs shall be located perpendicular to the trail
- Directional signs shall be prominently placed to increase visibility
- The signs shall be placed 5 feet above the trail surface, and if a secondary sign is mounted below maintain 4 feet above trail surface
- Signs shall be typically located 2 feet from the trail shoulder edge
- Sign scale should be in proportion to trail users and not exceed 7 feet in overall height
- Minor Directional Sign shall be brown as described in Section 2.12b, and additional lettering and Greenway logo shall be sharply contrasting bright white
- All directional sign lettering and Greenway logo shall be a minimum of 2 inches high and shall be clear, simple, and easy to read from a distance
- Signage corners shall be rounded, and 2 inches clear should be maintained around sign perimeter
- Sign panels shall not exceed 4 square feet in area



j.3 Information Standards

All Minor Directional Signs shall include the following information:

- Distance to select major destinations along the Greenway and public uses in the surrounding areas
- Directional arrows
- Arroyo Simi Greenway logo
- Minor Directional Signs - Location Identification signs should have National Parks symbols depicting uses such as picnic areas and restrooms at select key locations

j.4 Locations

Minor Directional signage should be located at:

- Trail and road intersections
- Greenway trail and existing or proposed pathway intersections in parks and/or rest areas

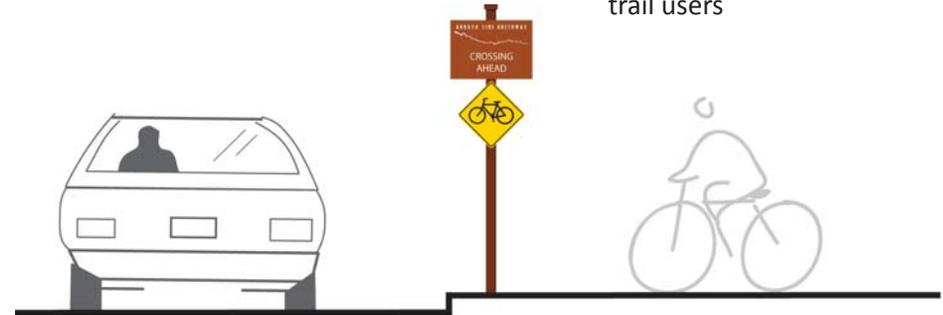
k. Roadway Approach Signs

Roadway approach signs shall be used to notify motorists of the upcoming Greenway crossings. These signs shall be designed with the same elements used in the Greenway signage program to repeat and reinforce a unifying sign theme. For motorists, a sign reading “Arroyo Simi Greenway Crossing Ahead” along with the Greenway logo helps to both warn about the crossing and promote use of the trail itself, and they shall have the same dimensions as standard City median roadway approach signs.

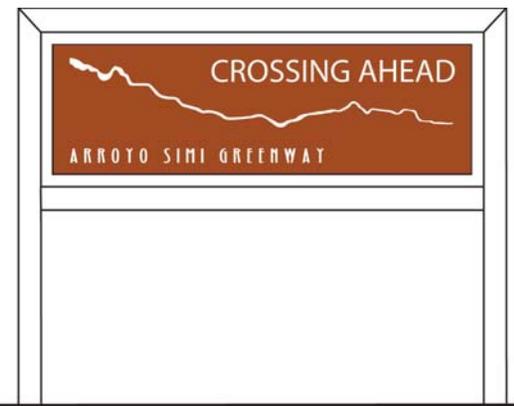


Two variations are presented in this section:

1. **Curbside Roadway Approach Signs**, used to warn drivers to slow down and yield to crossing trail users



Curbside Roadway Approach Sign



2. **Median Roadway Approach Signs**, developed because medians often occur on very wide roads and drivers in far lanes might miss the Curbside Sign. The Median Roadway Approach sign should be used along with a Curbside Roadway Approach Sign whenever feasible to alert drivers in all lanes of the upcoming crossing

Median Roadway Approach Sign

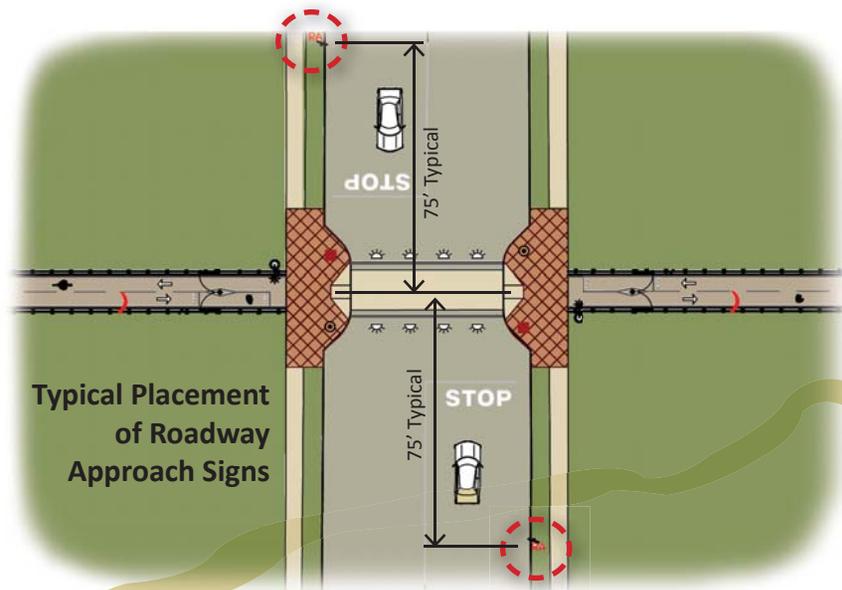
k.1 Materials Standards

- Median Roadway Approach Signs shall be similar to the City of Simi Valley's **Advance Street Name Sign**
- Curbside Roadway Approach sign panels shall be made of recycled aluminum with reflective paint for lettering and logo
- Curbside Signs new posts shall be made of recycled aluminum finished to match all other Greenway signs

k.2 Signage Standards

The following standards are for Curbside and Median Roadway Approach Signs

- The sign color palette shall be brown with white lettering and logo
- Roadway approach signs shall be the same throughout the Greenway
- Roadway Approach Signs shall not be visually obstructed by existing or proposed landscaping, signs, utilities, or landscaping
- All Roadway Approach Signs shall be located on the side of approaching traffic; perpendicular to direction of travel
- Signage corners shall be rounded
- Roadway Approach Signs shall include a Greenway logo and text and/or graphic indicating Greenway crossing ahead
- Roadway Approach Signs shall be located at all at-grade Greenway crossings, approximately 75 feet from center of Greenway crosswalks or before a turn lane which is not turning on to the Greenway crosswalk
- Roadway Approach Signs shall be located on roadways that turn on to a Greenway crosswalk, approximately 5 feet from stop bar or crosswalk where feasible



k.3 Median Approach Sign Standards

- The Median Sign shall be placed in the middle of an existing or proposed median
- Median Signs shall be located in approximately same distance from the crossing as the Curbside Sign

k.4 Curbside Approach Sign Standards

- Curbside Signs shall be placed 1 to 2 feet from the back of the curb
- Curbside Signs may be placed on own sign post, or can be affixed to an existing light or traffic sign post
- Curbside Signs should incorporate a Standard Bicycle Warning Sign (*refer to Traffic and Safety Signs, Section 2.12I*)

I. Traffic and Safety Signage

Standard traffic and safety signage will be used throughout the Greenway in order to state rules and regulations of the trail and to clearly direct all trail users, and will primarily be located at trail intersections and road crossings.

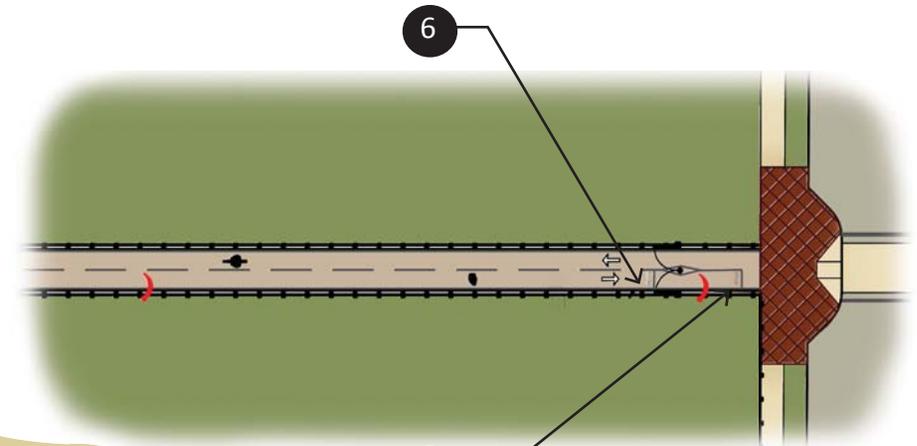
These basic signs and notices give concise, easy to understand expectations of trail users, to trail users. The result is a recreational experience that is safe and enjoyable for everyone.

Traffic and Safety Signage along the Greenway shall conform to the Manual of Uniform Traffic Control Devices Part 9, Traffic Controls for Bicycle Facilities (MUTCD), and Caltrans Chapter 1000 of the Highway Design Manual. Trail striping and painted bars are addressed in Trail Striping Standards, Section 2.5.5.

I.1 Information Standards

Traffic and Safety Signage along the Greenway shall include the following information:

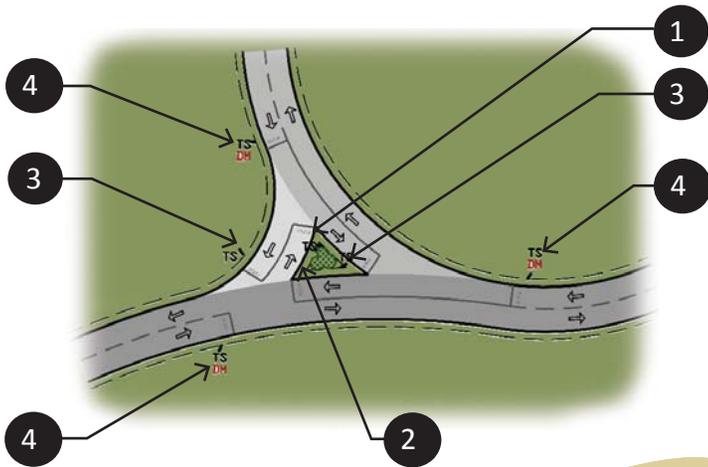
- Clear descriptions of trail rules and regulations, such as dog leash and speed limit regulations
- Bicycle traffic control signs where required
- Warning signs about potential conflicts such as trails crossing or converging
- Signs encouraging uniform trail user etiquette (“Bikes Yield to Peds”)
- When pathways intersect the Greenway trail system, signs shall be provided that indicate that Greenway trail users have the right-of-way



Typical Placement of Traffic and Safety Signage at Entry (refer to legend on following page)

I.2 Signage Standards

- Be located at least 2 feet (3 feet preferred) from the edge of the paved surface (except where not feasible, such as Drainage Trails)
- The signs shall be placed 5 feet above the trail surface, and if a secondary sign is mounted below, maintain 4 feet above trail surface
- All signs shall be oriented so as not to confuse trail users or motorists
- Traffic and Safety Signage shall incorporate a steel sign post, 4 inches square, consistently throughout the Greenway
- Traffic and Safety Signs shall be consolidated in order to reduce clutter, and is encouraged on the same post as Minor Directional Signs where possible. Depending on trail use, the signs could be located on top of or behind a Minor Directional Sign
- Text corresponding to the signs should be painted on the trail along with stop bars when applicable (*refer to Trail Striping Standards, Section 2.5.5*)
- Signage corners shall be rounded and have a rust colored border around the front panel (primary statement portion) of the sign



Typical Placement of Traffic and Safety Signage at Intersections (refer to legend at right)



1
Stop Sign



2
Slow Sign



3
Yield Sign



4
Slow & Minor Directional Sign



5
Stop & Minor Directional Sign



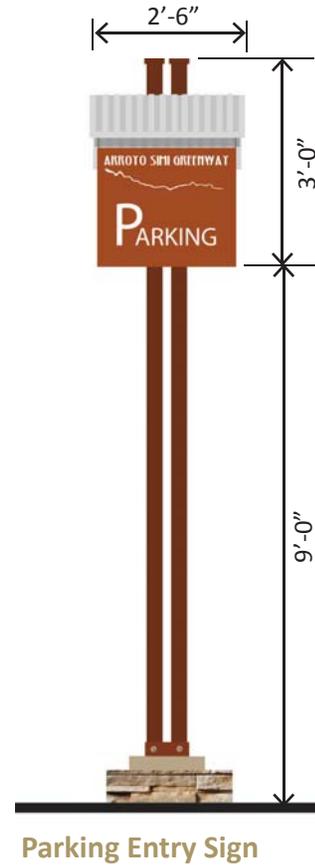
6
Slow, Minor Directional & Street Approach Sign

m. Trailhead Parking Signs

Trailhead Parking Signs should be used to mark the trailhead parking spaces. Trailhead Parking Signs shall integrate the Greenway design theme, and provide an icon for public parking associated with the Greenway trail system.

Trailhead Parking Signs should be located at all Trailhead/ Parking areas

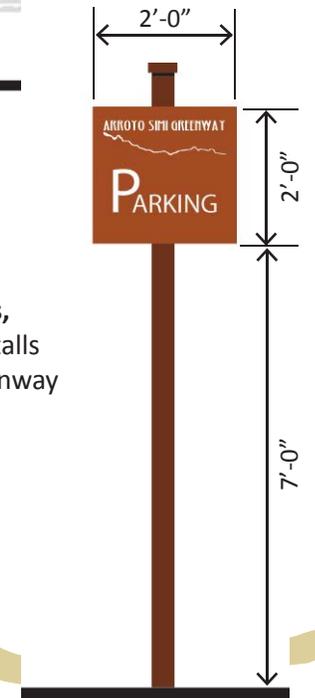
(Refer to Trailheads/Parking, Section 2.4)



Parking Entry Sign

Two types of parking signs are presented in this section:

1. **Parking Entry Signs**, used along the road at trailhead parking lot entrance to notify drivers of a Greenway trailhead parking area



Parking Space Sign

2. **Parking Space Signs**, used at individual stalls to designate a Greenway parking space

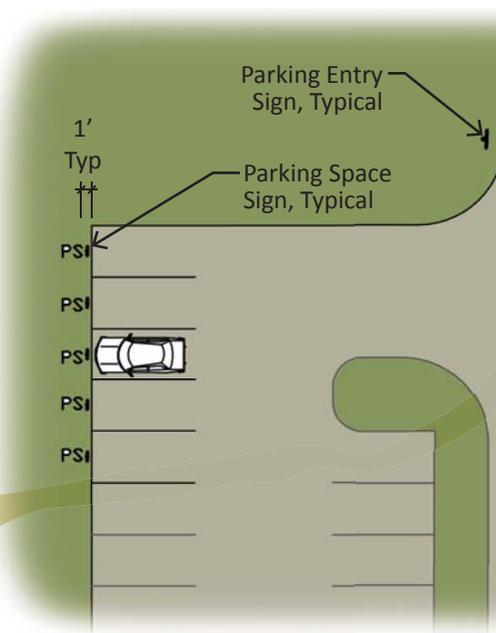
m.1 Materials Standards

- Parking Signs should have recycled steel posts painted to simulate weathered steel and reflect the design aesthetic of the Greenway signage
- Parking Signs should have a recycled steel sign panel and backing
- Parking Entry Signs have a corrugated Iron 'Roofing' Accent and a stone veneered *(refer to Masonry Standards 2.12d)* or concrete base

m.2 Signage Standards

- As a key element of the trailhead scene, the Trailhead Parking Signs shall clearly identify parking for the Greenway when feasible
- The same materials, method of construction, color scheme, and graphic theme shall be used on all Trailhead Parking Signs to unify the Greenway
- Parking Signs shall contain the Arroyo Simi Greenway Logo, and lettering should be 2 inches high minimum
- Parking Sign background shall be painted brown to simulate weathered steel as described in Section 2.12b, and additional lettering and Greenway logo shall be sharply contrasting bright white with a reflective finish
- Sign lettering shall be a minimum of 3.5 inches high, excluding the "P" which shall be 8 inches high
- Maintain a minimum of 18 inches clear on top and sides, and provide a minimum of 4 inches clear on the bottom of all Trailhead Parking Signs
- Signage corners shall be rounded

Typical Placement of Trailhead Parking Signs

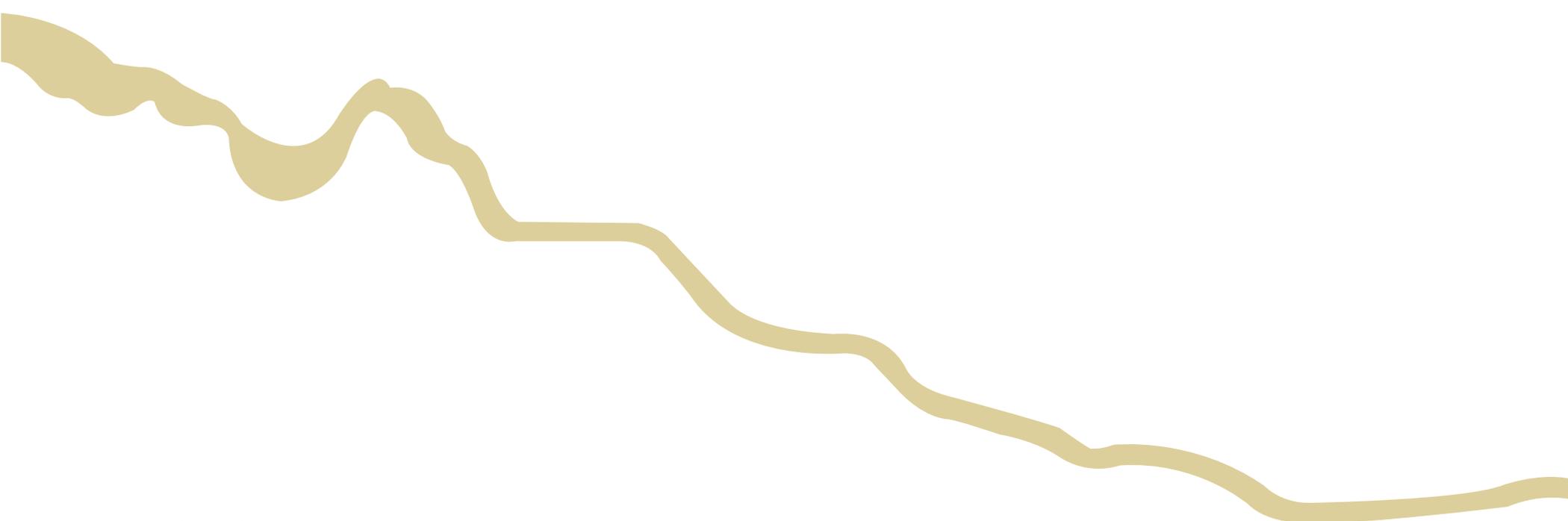


m.3 Parking Entry Sign Standards

- Locate Parking Entry Signs at the parking lot entry in clear view from the road
- Signs shall be located perpendicular to the road and facing the direction of travel which is entering the parking lot
- When possible, Parking Entry Signs shall be lit at night

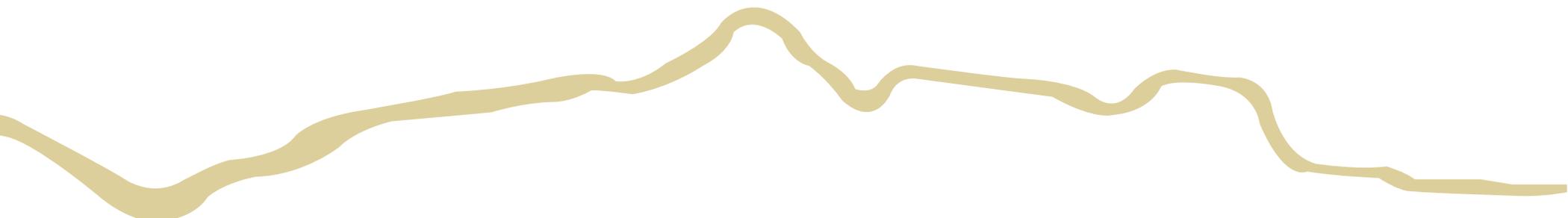
m.4 Parking Space Sign Standards

- Use Parking Space Signs to clearly designate Greenway parking spaces when feasible
- Parking Space Signs should be centered on the parking space and set back approximately 1 foot from the designated space when feasible



2.13

Fencing and Walls



DEV. PLAN SYMBOL	SECTION	CONTENTS	PAGE
N/A	2.13.1.....	Sustainable Elements	2.13-2
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N/A	2.13 c.....	Rhino Gates	2.13-14
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2.13 FENCING AND WALLS

This fencing and walls section was established to promote design character, define reasonable boundaries, and enhance trail user safety and property owner privacy. These standards consider preserving views and improving the appearance of the Greenway. Aesthetically pleasing fencing and walls will help to provide a safe and unifying link throughout the Greenway. Uniform fence and wall heights and styles will significantly improve the aesthetic environment for trail users.

The selections depicted in this section are examples and options of fencing and walls that meet the Arroyo Simi Greenway design aesthetic, but final selections may vary based on unforeseen situations that may occur after this Specific Plan was created. Final fencing and walls chosen at time of construction shall meet the design intent of the selections shown in this section in regard to type, scale, and aesthetic look and feel of materials.

Fencing and Walls Intent

- Provide consistent fencing and walls to develop a uniform and predominant design element for all trail segments along the Greenway
- Minimize the number of fence types along the Greenway, and use them only where necessary
- Develop an attractive fencing and walls program that meets the aesthetic character
- Specify public fencing and walls that provide for relatively unobstructed viewing
- Use materials that are not maintenance intensive and are graffiti resistant
- Regulate the height of fences and walls to maintain sight lines

2.13.1 Sustainable Elements

To the extent feasible, the following sustainable elements shall be used for fence and wall design and construction:

- Incorporate recycled steel for gates and fencing
- Consider recycled plastic or composite fencing
- Use reclaimed materials or recycled concrete for Greenway walls



Recycled concrete walls could be used in parks and overlooks

a. Greenway Fencing

There are four types of fencing proposed along the Greenway:

1. **Entry and Park Fencing** - Intended to direct Greenway access, protect trail users, prevent unauthorized entry, and unify the appearance of parks, overlooks and entries
See Section 2.13a.2
2. **Trail Fencing** – Used to direct trail users away from potential dangers and prevent unauthorized access
See Section 2.13a.3
3. **Chain Link Enhancements** - Potential improvements to existing chain link fencing
See Section 2.13a.4
4. **Property Fencing** – Located on property adjacent to the Arroyo Simi Greenway and suggested to be installed when the property is improved or redeveloped
See Section 2.13a.5

a.1 Greenway Fencing Standards

- Locate fences only where needed for safety and limiting access, and they shall be set back a minimum of 2 feet from the edge of trail when feasible and outside of the Arroyo Simi channel
- Step fences on sloped ground when necessary
- Use graffiti resistant fences
- Maintain a clear sight triangle and do not impede vision at any trail or road intersection
- Ensure that fence post footings are of adequate design to resist wind loads, and that fence materials will withstand the elements
- Use joinery and hardware that allows fences to be dismantled and reassembled for repair, replacement or recycling
- Where a fence intersects a wall or another fence type or fence with varying heights, they should gradually step and/or be separated by a column
- If one side of a fence has a more finished appearance than the other, the side of the fence with the more finished appearance shall face the Greenway trail or amenity

Fencing should compliment the surrounding landscape



Provide a landscape buffer to separate the fence from the trail



- Where feasible, a landscape buffer containing appropriate landscape plant material should be used to separate the fence from the trail (refer to Section 2.10 for additional information)
- Consider using alternatives to fencing such as barrier landscape materials such as boulders or planting (refer to Section 2.10 for additional information)
- Fencing shall consist of neutral colored and non-reflective materials which blends with the natural surroundings
- Temporary construction fences necessary for Greenway development shall be exempt from the provisions of this section

a.2 Entry and Park Fencing

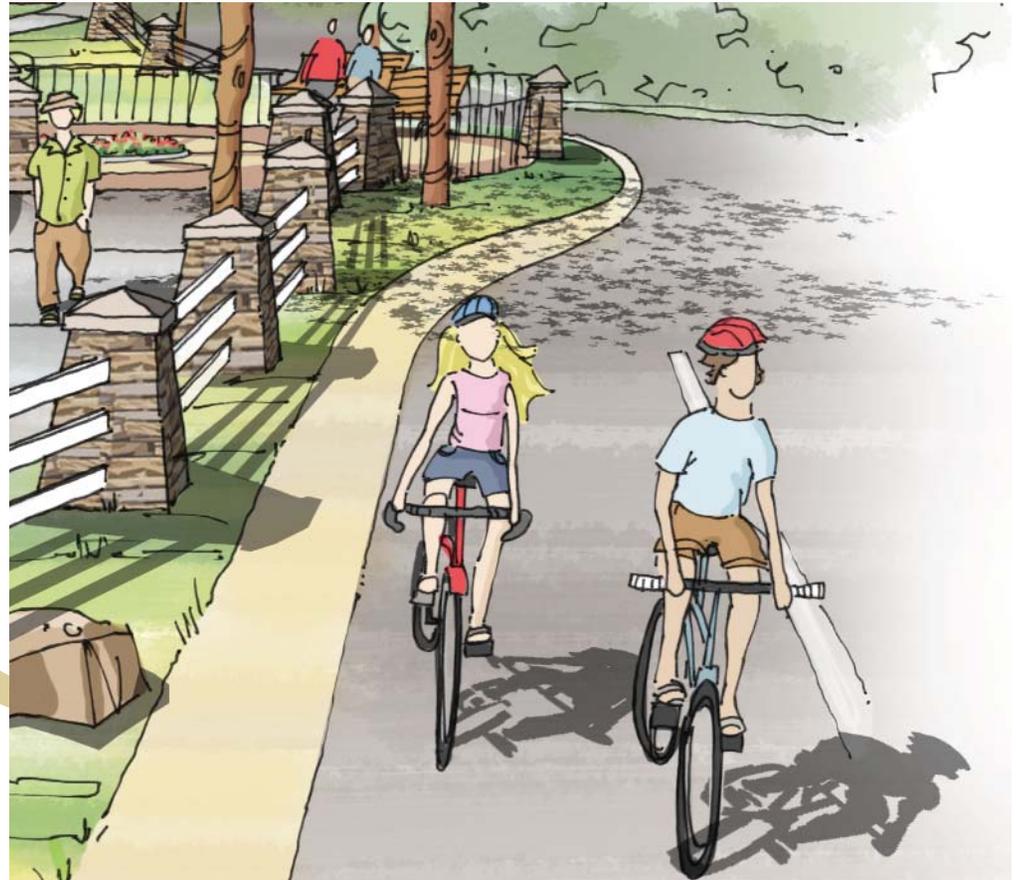
Entry and Park Fencing shall be located at entries to prevent unauthorized vehicles from accessing the trail, prohibit use during potential seasonal flooding, and to define the trail area leading up to at grade crossings. Entry and Park Fencing should be used in parks and overlooks to prevent trail users from entering the trail in uncontrolled locations, and direct trail users to specific locations, which have clear site lines and good visibility in all directions.



Example of Entry and Park Fencing with Columns

There are two variations of Entry and Park Fencing used within the Greenway:

1. Entry and Park Fencing with Columns – Used in conjunction with rock columns at entries and along main trails and pathways within parks
2. Entry and Park Fencing without Columns – Located along park perimeter areas that are less visible to trail and park users



Use fencing at rest areas/overlooks to direct trail users to specific locations which have clear site lines and visibility

Entry and Park Fencing shall incorporate Fencing Standards from Section 2.13a.1, and also include the following:

- Entry and Park Fencing shall consist of a 2 or 3 rail vinyl with a matte finish or concrete fence
- Fences shall be 42 inches high
- Columns shall be designed in proportion to fence and surrounding area, and include a cap of either natural stone or precast concrete with an integral color to match column
- Columns shall consist of stone masonry similar to gateway signage and be located approximately 10 to 40 feet on-center depending on location, function and visibility (*refer to Masonry Standards in Section 2.12d.6 for additional information*)
- Entry and Park Fencing shall be used in conjunction with removable bollards and rhino gates to direct and control access to the Greenway (*refer to Section 2.13c for information about rhino gates and Section 2.14c for additional information about bollards*)

See Parks, Section 2.8, Project Entries, Section 2.11, and Fencing and Walls, Section 2.13b.2, for additional information



Entry and Park Fencing should reduce the need for future maintenance and consist of a durable vinyl (top) or concrete (right)



a.3 Trail Fencing

Trail Fencing will be used to direct the users away from potential hazards, such as a sharp drop off. Trail fencing will also be used to prohibit unauthorized access within the Arroyo Simi Greenway, such as across from Darrah Volunteer Park on the south side of the Arroyo Simi where a fence is required to separate the Ventura County Watershed Protection District (VCWPD) operations yard from the proposed trail.

In general, there should be minimal trail fencing along the Greenway. When feasible, provide natural edge protection such as native barrier vegetation or boulders as an alternative to fencing.



Trail fencing should allow for maximum transparency and viewing of the Arroyo Simi

Trail Fencing will be used to direct the users away from selected sites, resources and potential hazards



Trail Fencing shall incorporate Fencing Standards from Section 2.13a.1, and also include the following:

- Trail fencing shall consist of painted metal, galvanized or concrete posts; painted metal or galvanized top and bottom rails; and galvanized mesh or pickets or other approved transparent design
- Trail fencing shall typically be 42 inches high, and 6 feet high when security issues exist
- Trail fencing shall not be installed in the Arroyo Simi channel
- Trail fencing shall be only installed as necessary in order to preserve the open feeling along the Greenway
- Consider placing a smooth wide rub-rail at 3 feet high on fences along trails without an adjacent 2 feet wide shoulder
- Trail fencing shall incorporate natural and receding colors such as brown, black, grey or green
- Trail fencing shall allow for clean unobstructed viewing
- Trail fencing shall be designed to work with VCWPD operations and management needs, and provide sliding gates where necessary



Trail Fencing shall consist of a mesh grid (far left) or picket fencing (left) to reduce future maintenance

a.4 Chain Link Enhancements

There are many areas along the Arroyo Simi that have existing chain link fencing installed. Some chain link fencing has recently been used along a new trail segment between the proposed S-Curve Park and Yosemite Avenue. Chain link has been used in these areas because it is very sturdy, durable and low maintenance. The use of new chain link fencing is only allowable within the channelized portions of the Arroyo Simi in non-visible portions of the Greenway.

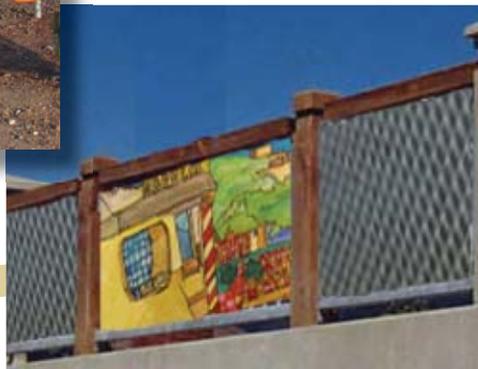


Plant climbing vines to cover the chain link



Add public art or cut outs along existing chain link fencing to increase visual interest and reflect the character of neighboring land uses

Cover existing metal posts and rails to be similar to Trail Fencing



In highly visible areas or areas with high anticipated use, if the existing chain link fencing is retained, it shall be customized to meet the Greenway design aesthetic with suggested enhancements listed within this section. As the existing chain link fencing deteriorates, new trail fencing described in 2.13a.3 shall replace the chain link.

Consider the following ideas for chain link fencing enhancement:

- At the Drainage Trail entries, replace existing chain link fencing with trail fencing along the street and consider replacing the first 100' along the trails where visible from the entrance (*refer to Trails, Section 2.5d*)
- Cover existing posts, top and bottom rails with a painted metal or galvanized hollowed post similar to Trail Fence described in Section 2.13 a.3
- If chain link is required, consider replacing with a natural colored vinyl coating whenever feasible
- Plant climbing vines and landscaping to cover the chain link where feasible (*refer to Landscaping, Section 2.10 for additional information*)
- Add public art panels or cut outs along the chain link to increase visual interest (*refer to Public Art, Section 2.15 for additional information*)
 - Panels and/or cut outs shall be located between the Greenway Banners
 - Panels and/or cut outs shall be placed in the center of the fence posts
 - Panels and/or cut outs shall be graffiti resistant and reproducible if replacements are needed

a.5 Property Fencing

The following information pertains to fence installation and maintenance on residential and commercial properties adjacent to the Arroyo Simi Greenway. As private property adjacent to the Greenway is improved or redeveloped, Property Fencing shall be required.

Property fencing shall be built entirely on private property, and maintenance and construction will be the responsibility of the private property owner.



Provide a column between varying fence types along the Greenway

Property fencing shall incorporate Fencing Standards from Section 2.13a.1, and also include the following:

- Perimeter private property lines shall be fenced with privacy fencing
- Berms shall not be permitted for the purpose of achieving a higher fence than would otherwise be permitted
- Where a property fence intersects another fence type along the Greenway, a column shall be provided
- Fences shall be stained or painted a uniform natural color on the Greenway side if necessary
- Property fences shall not contain additional security apparatuses such as barbed wire, security wire, sharpened tops or electric fencing
- Gates are not permitted to enter onto the Greenway due to the need to control trail system entry during times of flooding
- These standards shall not conflict with those of fences and walls which serve as swimming pool barriers

Property Fencing should consist of a concrete or vinyl privacy fence

a. GREENWAY FENCING

There are special fencing provisions for multi-family, multi-use developments and businesses along the Arroyo Simi to promote an open and inviting atmosphere along the Greenway. Property fencing shall incorporate applicable Fencing Standards from Section 2.13a.1 and 2.13a.5, and also include the following:

- Consistent transparent fencing when possible (refer to 2.13a.3 for examples)
- All new fences or repairs to fences which would result in a significant change of the appearance of the fence must be approved by the City of Simi Valley



Multi-family, multi-use developments and businesses shall construct transparent fencing along the Greenway



b. Greenway Walls

The standards in this section pertain to proposed or existing walls and bridge abutments. The Greenway Wall Standards also apply to proposed wall façade improvements, wall and fencing columns, and trash enclosures. These amenities shall incorporate the design aesthetic set by this section.



Greenway walls may include decorative façade elements such as decorative stone

There are two types of wall proposed along the Greenway:

1. Entry and Park Walls – Intended to direct Greenway access and unite the look of parks, overlooks and trail entries

See Section 2.13b.2

2. Retaining Walls – Used to contain slopes and facilitate road under crossings

See Section 2.13b.3



Entry walls and fencing at Sycamore Drive Community Center Trailhead