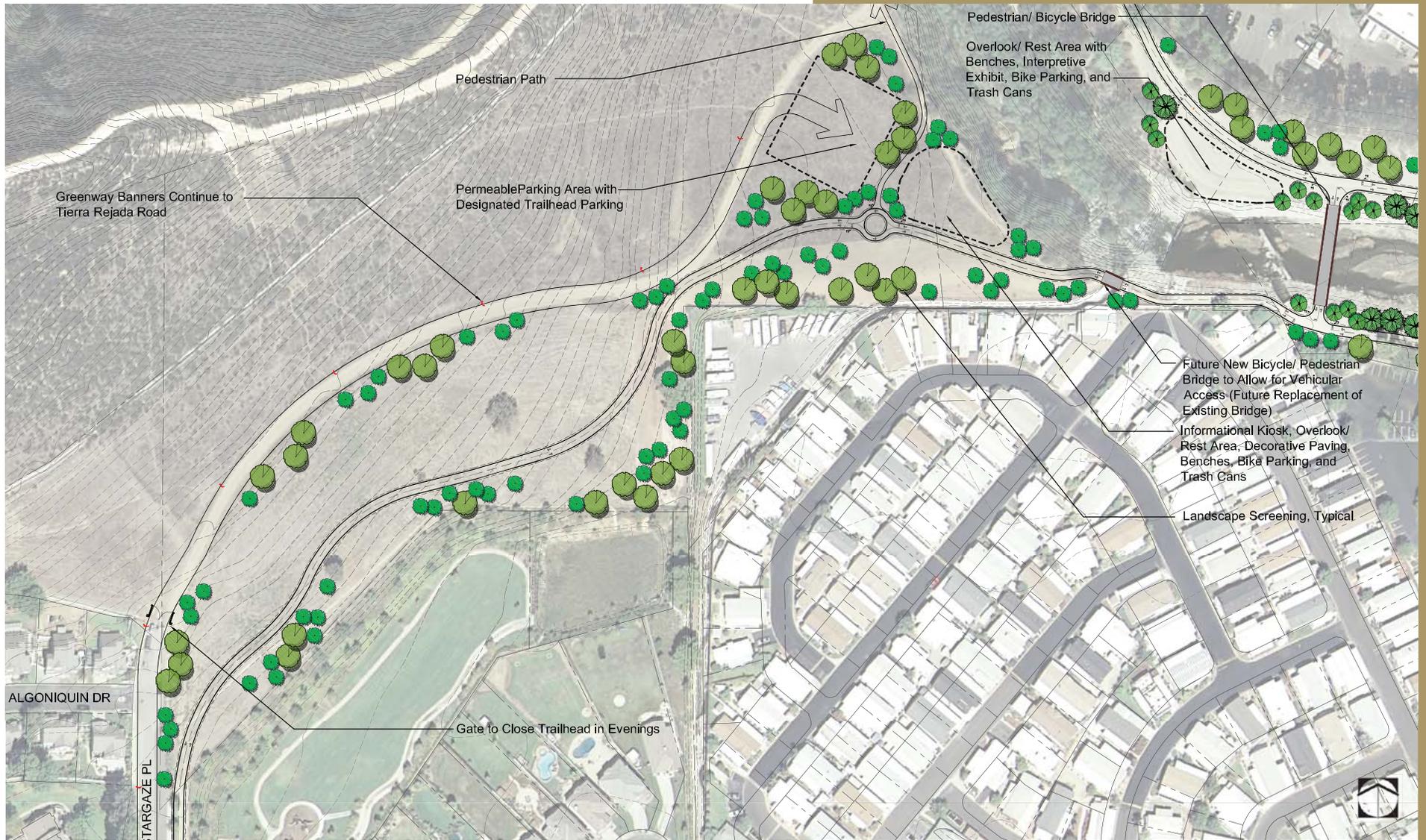


# Tierra Rejada Trailhead

plan view



a. TIERRA REJADA TRAILHEAD



b. RANCHO SIMI PARK TRAILHEAD

## b. Rancho Simi Community Park Trailhead

Rancho Simi Community Park Trailhead has been designed within Key Study Area 7. This trailhead has excellent adjacency to the Arroyo Simi and also ample existing amenities such as play equipment, restrooms and picnic areas. Because of the existing high volume of use in this park, parking space signs are not proposed for this trailhead. However, a Greenway Trailhead Parking Entry Sign will be installed to highlight the Greenway parking potential (*refer to Section 2.12m Trailhead Parking Signage*).

The trailhead at Rancho Simi Community Park was proposed in the Visioning Study to be located at the existing southwestern parking lot and recently renovated ball fields. The Site Development Plan relocated it to the parking lot adjacent to Erringer Road based on better proximity to the Arroyo Simi and the fact that the southwest parking lot usage for games is likely to be high on the weekends when trailhead parking will be most used.

Rancho Simi Community Park Trailhead incorporates all standards from Section 2.4.1, and also includes the following:

- Guide trail users to the existing amenities within the park and to the trail
- Tie the trailhead into the proposed park expansion areas and primary gateway
- Increase the size of the existing parking lot to provide additional Greenway parking, and expand northwards towards the Arroyo Simi Greenway



Location of the Rancho Simi Community Park Trailhead



**1**  
View of existing Greenway entry looking east towards Erringer Road



**2**  
View of the existing picnic areas looking southwest from the trail

Rancho Simi Community Park Trailhead  
plan view



Primary Gateway Monument  
(Two-Column), Low Walls,  
Rhino Gates & Removable Bollard

Picnic Tables, Typical

Bike Racks and Trash Cans

Multi-Use Trail into Park

Greenway Banners

Trailhead Parking with  
28 new spaces

Entry Plaza with Decorative  
Paving, Informational Kiosk &  
Bike Racks

Parking Entry Sign

ERRINGER ROAD



b. RANCHO SIMI PARK TRAILHEAD



c. COMMUNITY CENTER TRAILHEAD

## c. Sycamore Drive Community Center Trailhead

The Sycamore Drive Community Center Trailhead has been designed as Key Study Areas 9 and 9a. The adjacency to the Arroyo Simi and ownership by the Rancho Simi Park and Recreation District provides the opportunity to create a trailhead and primary gateway off of Sycamore Drive. The Community Center offers many recreational programs to the community of Simi Valley and has existing public restrooms.

The Sycamore Drive Community Center Trailhead incorporates all standards from Section 2.4.1, and also includes the following:

- Guide trail users to the existing amenities within the Community Center and to the trail
- Tie the trailhead into the proposed primary gateway entrance
- Reconfigure the existing parking lot to allow for a plaza that does not necessitate driveway crossings to enter the trail
- Relocate the existing bus stop into the proposed plaza area
- Widen the sidewalk along Sycamore Drive from Heywood Street to the trailhead because there is sufficient room to provide for high pedestrian traffic



Location of the Sycamore Drive Community Center Trailhead

looking west

1

Group of photos show the existing parking lot



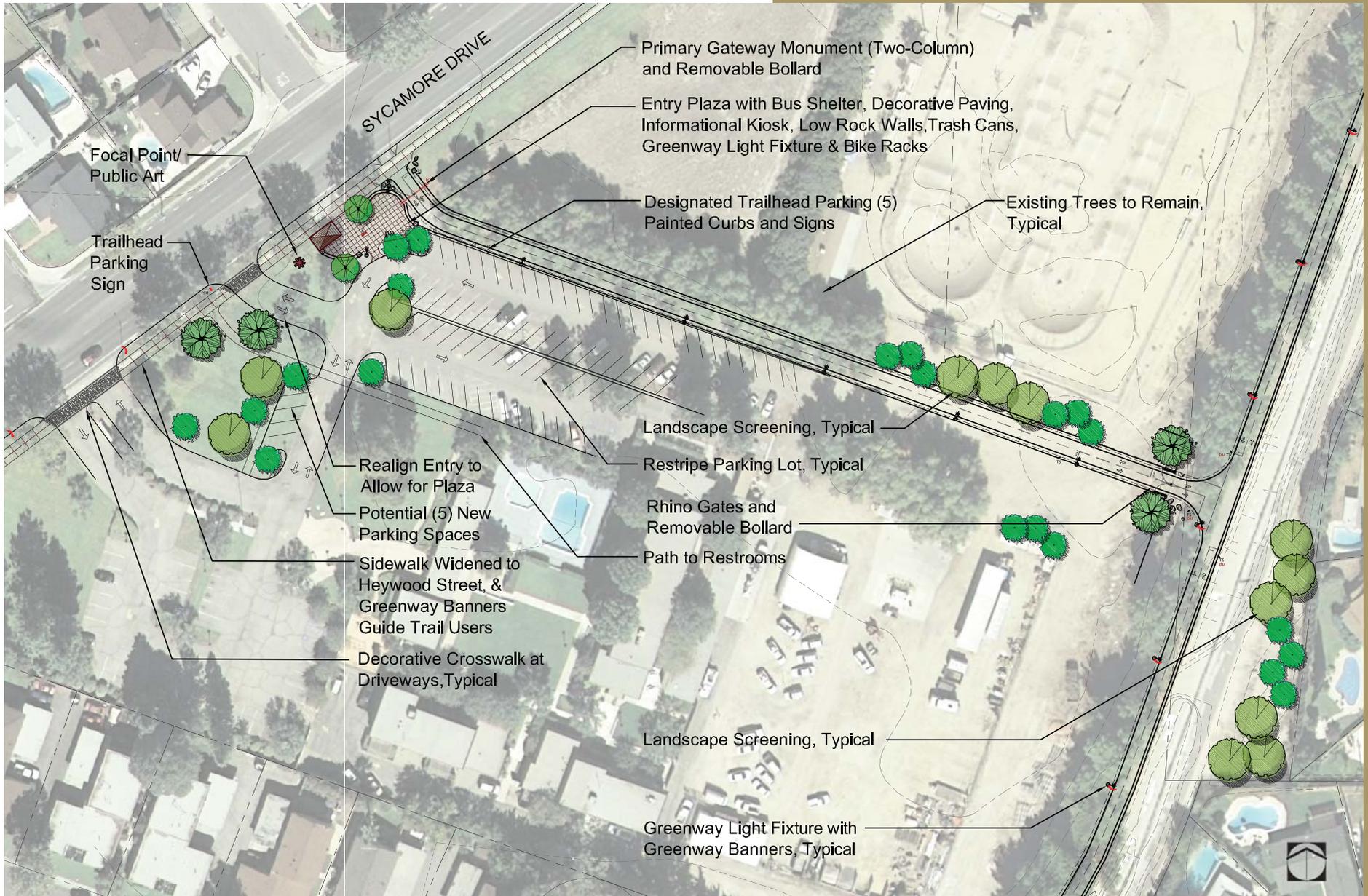
looking east

2

Looking east to the proposed connection to the existing trail



*Sycamore Drive Community Center Trailhead*  
plan view



Focal Point/  
Public Art

Trailhead  
Parking  
Sign

SYCAMORE DRIVE

Primary Gateway Monument (Two-Column)  
and Removable Bollard

Entry Plaza with Bus Shelter, Decorative Paving,  
Informational Kiosk, Low Rock Walls, Trash Cans,  
Greenway Light Fixture & Bike Racks

Designated Trailhead Parking (5)  
Painted Curbs and Signs

Existing Trees to Remain,  
Typical

Realign Entry to  
Allow for Plaza

Potential (5) New  
Parking Spaces

Sidewalk Widened to  
Heywood Street, &  
Greenway Banners  
Guide Trail Users

Decorative Crosswalk at  
Driveways, Typical

Landscape Screening, Typical

Restripe Parking Lot, Typical

Rhino Gates and  
Removable Bollard

Path to Restrooms

Landscape Screening, Typical

Greenway Light Fixture with  
Greenway Banners, Typical



d. OTHER TRAILHEADS

## d. Other Trailheads

There are five other trailheads proposed along the Arroyo Simi Greenway that are not illustrated as Key Study Areas. These trailheads should be developed similar to the Key Study Area designs, and incorporate all standards from Section 2.4.1.



Locations of additional Greenway Trailhead/Parking Areas

### d.1 Sequoia Avenue Trailhead

- Located on the existing vacant dirt area on the southwest side of the Arroyo Simi and Sequoia Drive intersection
- This trailhead is envisioned to be similar to the Community Center Trailhead because it is a primary gateway and involves a new parking lot design

Sequoia Avenue Trailhead shall incorporate all of the standards from Section 2.4.1, plus include the following:

- New trailhead parking may also be used as parking for Vista del Arroyo Park, with a connection to the park through the proposed Greenway street under crossing
- New parking shall comply with all parking requirements specified in the Simi Valley Municipal Code



Looking east towards Sequoia Avenue

Looking west at the proposed trailhead/parking area





## d.2 Darrah Volunteer Park Trailhead

- A Key Study Area was developed for the Darrah Volunteer Park Expansion, but it did not include the trailhead (refer to Section 2.8b.3 for additional information)
- The trailhead is proposed on the east side of ball fields under existing power lines
- This trailhead is envisioned to be similar to the Tierra Rejada Trailhead because it is a new parking lot design tied to additional Greenway facilities

Darrah Volunteer Park Trailhead shall incorporate all of the standards from Section 2.4.1, plus include the following:

- Connect the trailhead to the proposed park expansion enhancements
- The parking lot shall include a 7' wide planter after every 15 spaces in a row, per the Simi Valley Development Code
- 20' x 30' long additional storage area for the youth baseball league should be incorporated into the restroom structure
- Restroom architecture shall match Greenway Craftsman style (refer to Section 2.14e)



Group of photos are looking north at the connection of the proposed trailhead to the existing parking





### Darrah Volunteer Park Trailhead plan view



### d.3 Rancho Santa Susana Park Trailhead

- The high visibility of this trailhead along Los Angeles Avenue provides an excellent opportunity to identify Greenway parking and access
- There are existing public restroom facilities and ample parking
- The existing traffic signal and wide sidewalks along Los Angeles Avenue provides an easy connection to the Greenway

Rancho Santa Susana Park Trailhead shall incorporate all of the standards from Section 2.4.1, plus include the following:

- Highlight the adjacency to the Amtrak/Metrolink station to trail users through informational kiosk and/or directional signage
- Provide a map on the informational kiosk to guide trail users to the Greenway
- Incorporate Greenway banners along the sidewalks to Greenway entrances



View looking south towards LA Avenue and the proposed trailhead/parking area





Bicycle event in the existing parking lot looking east

#### d.4 Corriganville Park Trailhead

- Corriganville Park contains the headwaters of the Arroyo Simi and links to many regional trails
- This trailhead should not be constructed until after the Arroyo Simi Greenway is open from Yosemite to Corriganville Park

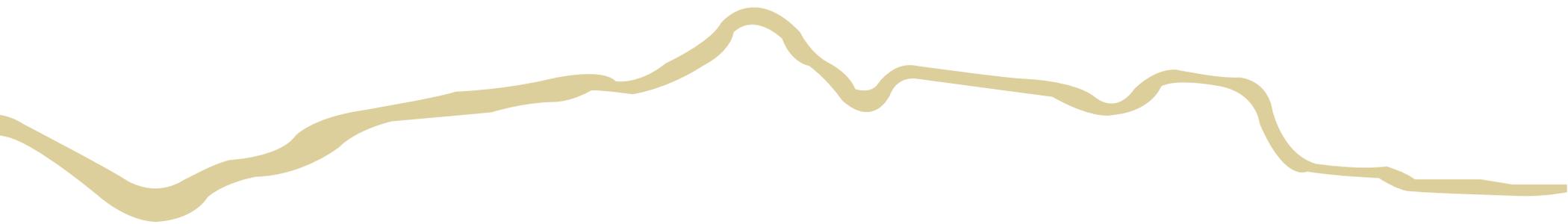
Corriganville Park Trailhead shall incorporate all of the standards from Section 2.4.1, plus include the following:

- Highlight the scenic vistas
- Provide directions to the regional trail system within the park
- Incorporate the park's rich film industry heritage into the interpretive program for the Greenway



2.5

# Trails Program



DEV. PLAN SYMBOL	SECTION	CONTENTS	PAGE
N/A	2.5.1	Sustainable Elements	2.5-3
N/A	2.5.2	Trails Key Study Areas	2.5-4
N/A	2.5.3	Greenway Trail Standards	2.5-6
N/A	2.5.4	New Trail Standards	2.5-8
N/A	2.5.5	Trail Striping Standards	2.5-10
	2.5 a	Existing Paved Trail to Remain	2.5-12
N/A	2.5 a.1	First Street Views, Key Study Area	2.5-14
	2.5 b	New Paved Trail – Type 1	2.5-16
	2.5 c	New Paved Trail – Type 2	2.5-20
	2.5 d	Drainage Trail	2.5-24
	2.5 e	New Drainage Sidewalk	2.5-26
	2.5 f	Widened Sidewalk	2.5-29
	2.5 g	Equestrian Trail	2.5-31
	2.5 h	Existing Dirt Trail to Remain	2.5-34

## 2.5 TRAILS PROGRAM

The Arroyo Simi Greenway is a multi-use corridor system that has a series of trails extending alongside the Arroyo Simi and its associated drainage channels. When complete, the pedestrian and bicycle trails will provide a safe and convenient Class I bike route through the middle of Simi Valley, and link to the City of Moorpark on the west and Corriganville Park on the east. The trail system will serve a combination of users including pedestrians, bicyclists, roller bladers, joggers, and equestrians and it will create a safe route for recreational and commuter users.

When designing or maintaining the shared use trails, safety shall be the highest priority and the needs of each group must be met in order to create a trail that minimizes user conflict and decreases potential impacts by the varied user groups. The various physical needs and abilities of trail users, including children, adults, the elderly, and persons with hearing, visual, or cognitive impairments must be accommodated. Trails must be designed to allow for universal access and they shall be readily accessible and usable by individuals with disabilities, and must meet all requirements of the Americans with Disabilities Act (ADA).

### Trails Intent

- Reduce potential user conflicts and ensure trail safety
- Design long lasting trails which require minimal trail maintenance
- Create shared use trails with sufficient sightlines and safe maximum speeds
- Ensure that the trail system is constructed with a paved surface sturdy and wide enough to safely accommodate multiple uses and maintenance vehicles
- Provide continuous and direct routes which link trail users to places of employment, recreational areas and centers of activity to allow for modes of transportation other than an automobile
- Conveniently connect the Greenway trail system to other modes of alternative transportation
- Consistently use similar design elements described in this section for the entire trail system to develop a unified Greenway
- Integrate and optimize existing aesthetic attributes in all aspects of trail design, and attempt to weave the trails into nature and adjacent parks whenever feasible

The Arroyo Simi Visioning Study dated October 23, 2007, described an enhanced trail route with Primary, Secondary and Connector trails. This system of trails is still envisioned; however a more defined trail alignment based on method of construction is necessary to begin to develop costs and materials for the Greenway.

*Refer to Site Development Plan, Section 2.1b for additional information*



**View of existing trail along the Tapo Canyon Drain looking north, note the aged fencing and asphalt condition**

There are nine types of trails proposed along the Arroyo Simi Greenway, and they are generally described by type of construction, and explained in detail within this section.

- **Existing Paved Trail to Remain**

Existing paved trail that will remain in place and may be widened and resurfaced as needed (typically found on the north side of the Arroyo Simi)

*Refer to Section 2.5a*

- **New Paved Trail - Type 1**

Existing dirt or gravel trail that will become paved (typically found on the south side of the Arroyo Simi)

*Refer to Section 2.5b*

- **New Paved Trail - Type 2**

Trail that is not on an existing dirt or gravel trail and requires additional site preparation and grading (such as those found east of Madera Road on the north side of the Arroyo Simi)

*Refer to Section 2.5c*

- **Drainage Trail**

Existing or proposed paved trail that is located along an existing drainage corridor (such as along Arroyo Simi west of Yosemite Avenue)

*Refer to Section 2.5d*

- **New Drainage Sidewalk**  
New paved sidewalk that is too narrow to be a Drainage Trail (such as located along 5th Street leading to Sinaloa Middle School - Key Study Area 4 and 4a)

*Refer to Section 2.5e*

- **Widened Sidewalk**  
Existing sidewalks to be widened to allow for anticipated Greenway trail connection use

*Refer to Section 2.5f*

- **Equestrian Trail**  
Existing dirt or gravel trail that will remain unpaved (typically Greenway areas adjacent to existing horse properties and equestrian facilities which connect to surrounding regional trail systems)

*Refer to Section 2.5g*

- **Existing Dirt Trail to Remain**  
Existing dirt or gravel trail that is expected to remain unpaved and continue to be used for maintenance access (typically found on the south side of the Arroyo Simi)

*Refer to Section 2.5h*

## 2.5.1 Sustainable Elements

Include sustainable materials and maintenance practices on trails whenever possible.

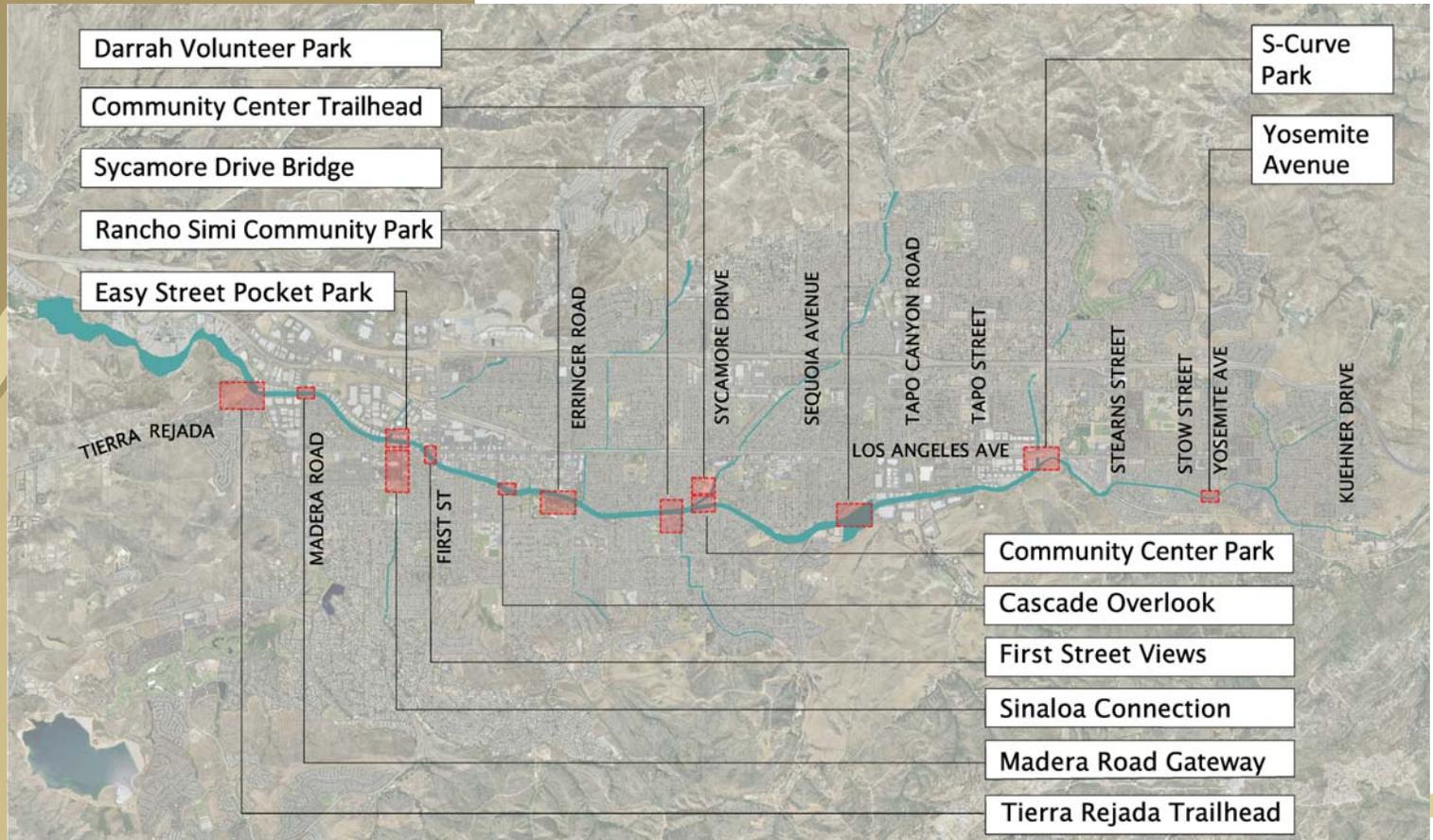
- Use reclaimed asphalt pavement (RAP) to remove and/or reprocess asphalt pavement materials for trails whenever feasible
- Low VOC paints shall be used for trail striping
- Use sustainable sealants for trails such as Solar Reflective Coating, Recyclable Paving Mats, Cementitious Surface Treatments, and/or Tire Rubber Modified Surface Seal whenever feasible

*Refer to Section 3.0 for additional information*

## 2.5.2 Trails Key Study Areas

Several trail types described within this section are shown in the Key Study Areas (KSAs), and all of the Key Study Areas include more than one trail type. In order to consolidate information and reduce repetition within the document, this section references plans and sketches from other sections within this Specific Plan.

Key Study Areas depicting multi-use trails



- **Tierra Rejada Trailhead: Key Study Area 1**
  - New Paved Trail - Type 1
  - New Paved Trail - Type 2
  - Existing Dirt Trail to Remain

*Refer to Trailheads/Parking, Section 2.4*

- **Madera Road Gateway: Key Study Area 2 and 2a**
  - Existing Paved Trail to Remain
  - Existing Dirt Trail to Remain

*Refer to Project Entries, Section 2.11*

- **Easy Street Pocket Park: Key Study Area 3**
  - Existing Paved Trail to Remain
  - New Paved Trail - Type 1
  - New Paved Trail - Type 2
  - Drainage Trail
  - Existing Dirt Trail to Remain

*Refer to Parks, Section 2.8a*

- **Sinaloa School Connection: Key Study Area 4 and 4a**
  - New Drainage Sidewalk

*Refer to Trails Program, Section 2.5e*

- **First Street Views: Key Study Area 5a**
  - Existing Paved Trail to Remain

*Refer to Trails Program, Section 2.5a*

- **Cascade Overlook: Key Study Area 6 and 6a**
  - Existing Paved Trail to Remain
  - New Paved Trail - Type 1
  - New Paved Trail - Type 2

*Refer to Overlooks/ Rest Areas, Section 2.9*

- **Rancho Simi Community Park: Key Study Area 7**
  - Existing Paved Trail to Remain
  - New Paved Trail - Type 1
  - Existing Dirt Trail to Remain

*Refer to Parks, Section 2.8b*

- **Sycamore Drive Bridge: Key Study Area 8 and 8a**
  - Existing Paved Trail to Remain
  - New Paved Trail - Type 1
  - Drainage Trail

*Refer to Under Crossings, Section 2.16c*

- **Community Center Trailhead: Key Study Area 9 and 9a**
  - Existing Paved Trail to Remain
  - New Paved Trail - Type 2
  - Widened Sidewalk

*Refer to Trailheads/Parking, Section 2.4*

- **Community Center Park: Key Study Area 10 and 10a**
  - Existing Paved Trail to Remain
  - New Paved Trail - Type 2

*Refer to Parks, Section 2.8a*

- **Darrah Volunteer Park: Key Study Area 11 and 11a**
  - Existing Paved Trail to Remain
  - New Paved Trail - Type 2
  - Equestrian Trail

*Refer to Parks, Section 2.8b*

- **S-Curve Park: Key Study Area 12 and 12a**
  - Existing Paved Trail to Remain
  - New Paved Trail - Type 2
  - Widened Sidewalk

*Refer to Parks, Section 2.8a*

- **Yosemite Avenue: Key Study Area 13**
  - Existing Paved Trail to Remain
  - Drainage Trail

*Refer to Project Entries, Section 2.11b*

### 2.5.3 Greenway Trail Standards

The following standards apply to all trail types within this section as applicable.



The trail system will serve a combination of users

- The Greenway trail shall incorporate American Association of Highway and Transportation Officials (AASHTO), California Department of Transportation (Caltrans) and the Uniform Building Code for bikeway trail design standards
- Clearly define which type of trail use has the right-of-way at shared-use trail and roadway intersections
- Trails should be accessible to all users regardless of age groups and abilities
- Maintain clear lines of sight and use low landscaping at trail and road intersections to maintain proper site distances
- Trail intersections and road crossings shall be aligned to create a right angle when feasible
- When nearing at grade road intersections or merging with other trails, incorporate slowing devices such as chicanes, roundabouts and/or painted warning stripes and signs
- Allow for a minimum of 2 feet clear distance on both sides of the trail between trees, poles, walls, fences, guardrails or other lateral obstructions
  - Where this clearance cannot be obtained, install signs and pavement markings to warn bicyclists of this condition
- Maintain 14 feet minimum of vertical clearance to obstructions over the trail, excepting under crossings where 8 feet minimum is permitted
- Provide a turning radius sufficient for a 40-foot long vehicle to make the turn while staying on the trail or shoulders
- The total width of the trail and shoulder shall be maintained through under crossings or bridges when possible

- Provide joggers with a separate trail when feasible (may consist partially or totally of the trail shoulders)
- Place drainage grates and utility covers out of the trail travel when feasible
- Install directional signage, traffic and safety signage, banners and mile markers per the Greenway Signage Program (refer to Section 2.12)
- Fencing should only be used as necessary to direct or limit trail access (refer to Section 2.13, *Fencing and Walls*)
- Removable bollards shall be used at all trail and road intersections to bar unauthorized vehicular entry (refer to Section 2.14c, *Site Furnishings Program*)
- Trails shall incorporate a minimum cross slope of 1% and a maximum cross slope of 2%, which drains to a filtration device located on the back side of the trail, to prevent unfiltered water from entering the Arroyo Simi channel
- Ensure a smooth surface on the trails to prevent water ponding
- Although most of the Greenway is relatively flat, areas such as under crossings should keep linear maximum trail grades between 5% and 8%, with grades greater than 5% kept to short distances
  - 5-6% is acceptable for up to 800 feet
  - 7% is acceptable for up to 400 feet
  - 8% is acceptable for up to 300 feet
  - Slopes exceeding 8% shall not be used because they are not acceptable for inexperienced bicyclists and are not compliant with Americans with Disabilities Act (ADA) guidelines, excepting where necessary such as at under crossings



**Greenway trails should be accessible to all users, regardless of age group or ability**



## 2.5.4 New Trail Standards

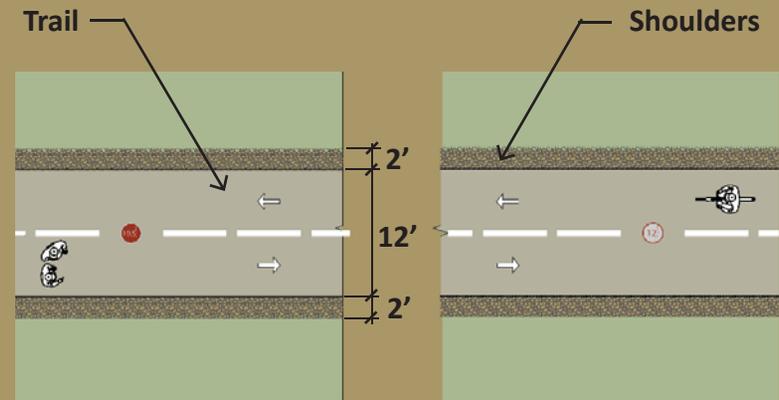
The following trails shall incorporate standards in Section 2.5.3 and also the standards within this section:

- **New Paved Trail - Type 1** (Section 2.5b)
  - **New Paved Trail - Type 2** (Section 2.5c)
  - **Drainage Trail** (Section 2.5d)
- Asphalt or concrete surfaces shall be used to withstand heavy trail use and the impact of maintenance vehicles and emergency vehicles, and be built to provide access for people with disabilities
    - Trails must be able to support the largest Flood Control vehicles, which weigh approximately 25 tons
  - Trails shall be located to align entries at safe intersections, and minimize mid-block crossings to the greatest extent feasible
  - Design shared-use trail and roadway intersections with flat grades when feasible
  - The minimum pavement depth of the trail will vary based on a soils report, and upon the ultimate decision to use asphalt or concrete paving
  - Trail entries should be located to be as visible as possible to passing vehicles on adjacent streets
    - At a minimum, provide safe stopping sight distances for both the vehicles on the roadway and the trail users
    - Consider the visibility of the pedestrian from the motorist's point of view
    - New planting, signs, furniture, and other Greenway appurtenances shall not be located where they will block the motorist's view of the pedestrian on the Greenway

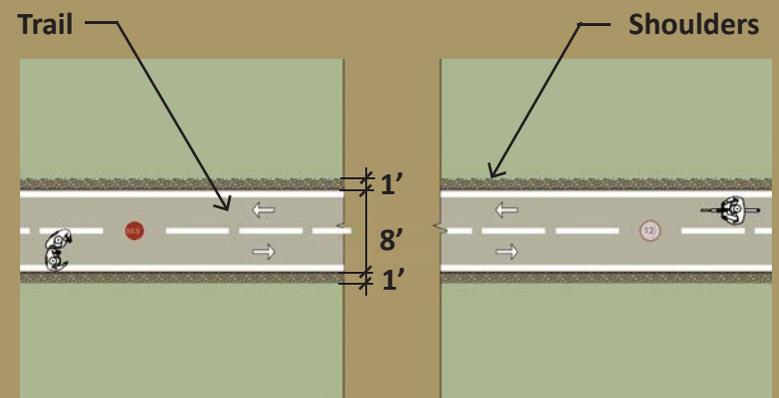
Designers must be aware of the various physical needs and abilities of all potential trail users



- The design speed for a shared-use trail is dependent on the terrain and the expected conditions of use, and trails should be designed to encourage bicycles to maintain speeds at or below the following speeds and turning radius:
  - Typical design speed for the Greenway along level or rolling terrain:
    - 20 mph design speed/  
90-foot turning radius
  - Long downgrades (steeper than 4% for longer than 500 feet):
    - 30 mph design speed/  
260-foot turning radius
  - When a smaller radius is needed, increase the pavement width by up to 4 feet on the inside of a curve to compensate for bicyclist lean or provide additional shoulder width as needed for maintenance or emergency vehicle access
  - Where required, no turning radius that prevents a vehicle up to 40 feet in length from making the turn without leaving the trail shall be allowed
- The typical trail width will be:
  - 12-foot wide trail
  - 2-foot wide aggregate base shoulders on each side of the paved trail and incorporate Best Management Practices (i.e., bioswales, permeable paving, etc.) on the back side of the trail to ensure that unfiltered water does not enter the Arroyo Simi channel
- The minimum trail width will only be permitted in locations where space is constrained such as Drainage Trails:
  - 8-foot wide trail
  - 1-foot wide aggregate base shoulders on each side of the paved trail if feasible
  - Incorporate Best Management Practices (i.e., bioswales, permeable paving, etc.) on the back side of the trail to ensure that unfiltered water does not enter the Arroyo Simi channel
  - 4-inch thick white striping located 6 inches from the trail edge



Typical Paved Trail



Minimum Paved Trail

## 2.5.5 Trail Striping Standards

Trail striping standards apply to the following trail types within this section:

- **Existing Paved Trail to Remain** (*Section 2.5a*)
- **New Paved Trail - Type 1** (*Section 2.5b*)
- **New Paved Trail - Type 2** (*Section 2.5c*)
- **Drainage Trail** (*Section 2.5d*)



White striping should occur on narrow trails where shoulders are minimal

- Trail striping dimension and placement must conform to the Manual On Uniform Traffic Control Devices (MUTCD)
- Directional arrows shall be used at trail entrances and intersections, including entries from an overlook or park, to clearly designate direction of travel
- Traffic and Safety Signs shall be incorporated in conjunction with trail striping as required along the Greenway (*refer to the Greenway Signage Program, Section 2.12I*)
- A 4-inch wide yellow center line stripe shall be used to create two lanes of travel, and it should be dashed when adequate passing sight distances exist, and solid when no passing is permitted such as through curves
- A 4-inch wide white edge striping should be used if no space for exists for shoulders
- Centerline markings are not required where there is not heavy use anticipated, such as at secondary entry pathways connecting into the main Greenway trail



Directional arrows should be used to clearly designate direction of travel



**Centerline markings are not required where low use is anticipated, such as at this entry off Easy Street**

- Bollards and other elements within the center of the travel lanes (such as a roundabout) shall have 4-inch wide yellow pavement markings around them
- Provide advance warning signs and pavement markings as necessary to alert and direct bicyclists to STOP and YIELD before reaching the intersection, especially on downgrades
  - Set back STOP and YIELD lines to provide for sight distance at all approaches to intersections, approximately 5-feet from the back of sidewalk or curb
  - Painted bars shall accompany at painted STOP or YIELD signs, and be placed as close to the intended stopping point as possible
- When two or more trails meet, anticipate the most used trail and develop a consistent system of priority for right-of-way (for example, trails with expected high use should have YIELD markings, and trails with the least amount of use should have STOP markings)
  - Also consider grades when developing the system of priority, and allow downhill traffic priority with YIELD markings, and place STOP markings on uphill trails



**Elements within the center of the travel lanes will have yellow pavement markings around them**

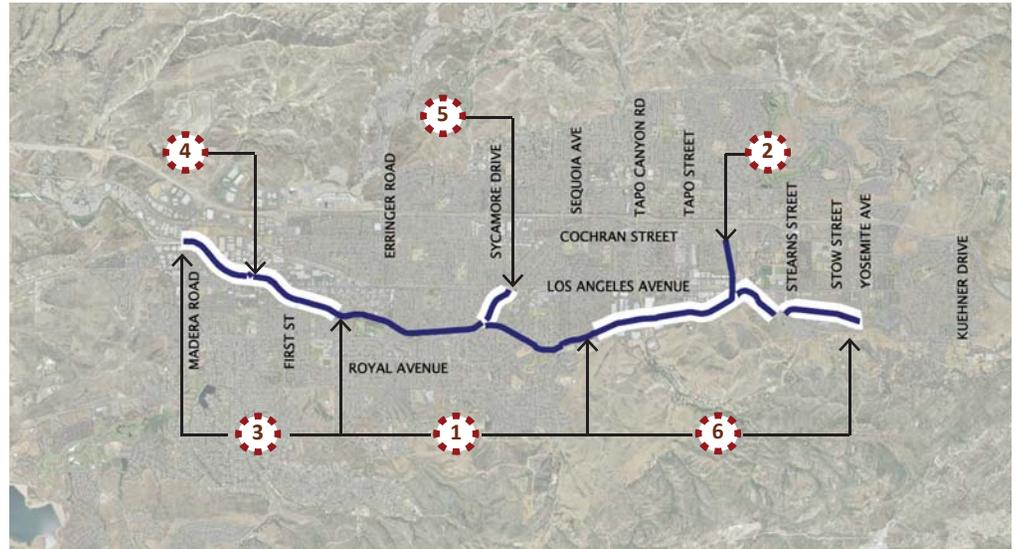
### a. Existing Paved Trail to Remain

There is a trail already existing on the north side of the Arroyo Simi Greenway, and it shall be used and enhanced. Existing Paved Trails to Remain are typically found on the north side of the Arroyo Simi from Madera Road to Yosemite Avenue. The design criteria within this section describe enhancing a typical existing paved trail.

In some areas, the existing trail is less than 12 feet in width. Existing Paved Trails to Remain designated as primary on the Site Development Plan (Section 2.1) should be widened to 12 feet where feasible, and be resurfaced and enhanced. Trails designated as secondary on the Site Development Plan do not need to be widened.



Locations of Existing Paved Trails to Remain



Secondary Existing Paved Trail to Remain alignments are located at:

- 1 Cascade Overlook to Darrah Volunteer Park to Yosemite Avenue
- 2 Kadota Fig Trail

Primary Existing Paved Trail to Remain alignments to be enhanced are located at:

- 3 Madera to the Cascade Overlook (located between First Street and Erringer Road)
- 4 Existing entry off Easy Street just west of the proposed pocket park
- 5 Tapo Canyon Drain (just east of the Sycamore Drive Community Center)
- 6 Darrah Volunteer Park to Yosemite Avenue

Existing Paved Trail to Remain shall include:

- Resurface or rebuild existing asphalt trail and re-grade 2-foot wide shoulders with an aggregate base and incorporate Best Management Practices (i.e., bioswales, permeable paving, etc.) on the back side of the trail to ensure that unfiltered water does not enter the Arroyo Simi channel where necessary
- Widening the trail to the typical 12' Greenway trail width where feasible
- Repaint yellow centerline stripe and arrows according to the Greenway Trail Striping Standards, Section 2.5.5
- New fencing and wall facades where applicable (refer to Section 2.13, Fencing and Walls)
- Where necessary, existing poor sightlines and troublesome intersections should be improved and speed reduction methods implemented
- Install landscape screening/enhancement along trails (refer to Landscaping, Section 2.10)
- Where indicated on the Site Development Plan, primary and secondary project entries shall be improved (refer to Section 2.1, Site Development Plan and Section 2.11, Project Entries)



Views of Existing Paved Trails to Remain west of the proposed S-Curve Park (top) and west of Darrah Volunteer Park (right)

Existing Paved Trail to Remain occurs throughout the Greenway, and there are examples of this trail type with each of the Key Study Areas. Refer to Section 2.5.2 for locations.

The Key Study Area described on the following pages is First Street Views, Key Study Area 5a.



Key Study Areas depicting Existing Paved Trails to Remain

### a.1 First Street Views, Key Study Area 5a

This segment of the Greenway includes an Existing Paved Trail to Remain on the north side of the Arroyo, and is very visible to motorists traveling along one of the City’s main arterial streets, Los Angeles Avenue.

The Key Study Area is located within a commercial zone and has very visible parking lots adjacent to the Arroyo Simi. The adjoining properties have chain link fencing along the property lines, and there is metal tube post and rail fencing existing along the trail. There are expansive blank concrete retaining walls on the north side of the trail with minimal landscaping.

Enhancing the views from Los Angeles Avenue will significantly improve how the public identifies with the Greenway, and greatly aid in promoting use of the trail system. Improvements in this Key Study Area include:

- Trail improvements (refer to *Greenway Trail Standards, Section 2.5.3, and Trail Striping Standards, Section 2.5.5*)
- First Street and Los Angeles Avenue bridge improvements and under crossing enhancements (refer to *Bridges, Section 2.6 and Under Crossings, Section 2.16*)
- Landscape screening/ enhancement and habitat enhancement (refer to *Landscaping, Section 2.10 and Arroyo Channel Improvements, Section 2.16*)
- Enhanced wall facades and fencing (refer to *Section 2.13, Fencing and Walls*)



Location of First Street Views



View looking west towards Los Angeles Avenue



View looking east from Los Angeles Avenue

*First Street Views*  
view looking east from LA Avenue



AFTER



BEFORE

a. EXISTING PAVED TRAIL

## b. New Paved Trail - Type 1

The existing dirt or gravel trail along the south side of the Greenway has been used for years by equestrians, and is already graded and compacted to facilitate a paved trail. The following design criteria within this section describe typical existing dirt or gravel trail that will become paved.

The south side of the Greenway is envisioned to become the most used trail along the Greenway because of its connection potential, ease of travel, afternoon shade, and most of the new south side trail is designated as primary on the Site Development Plan (Section 2.1).



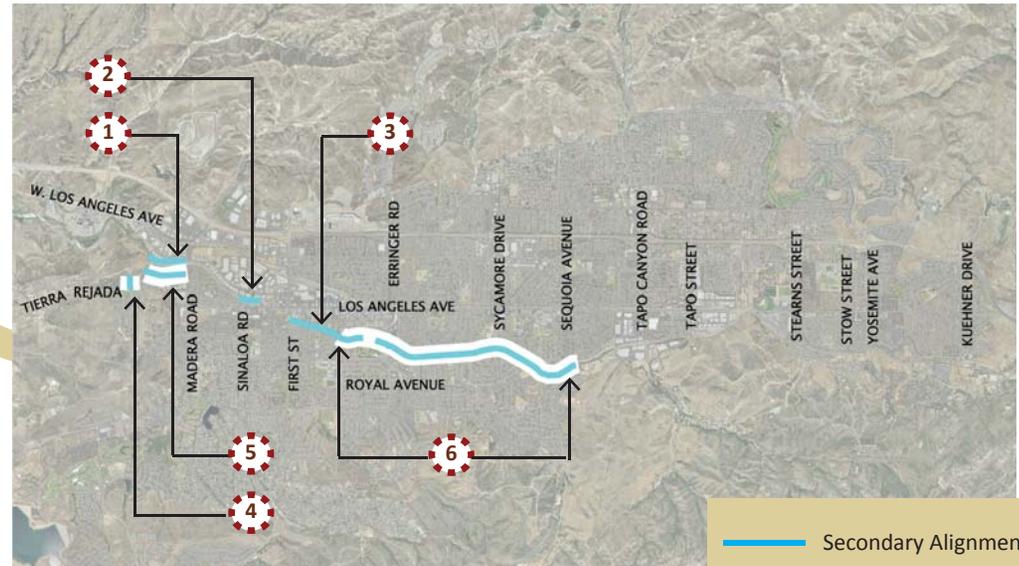
Example of a New Paved Trail - Type 1, looking west from the north side of Vista del Arroyo Park

Secondary segments of New Paved Trail - Type 1 alignments are located:

- 1 North side of the Arroyo Simi from Madera Road to the proposed New Paved Trail - Type 2 leading to the City of Moorpark, located just north of the Tierra Rejada Park
- 2 Segment just south of the proposed Easy Street Park from Sinaloa Road to the proposed Drainage Trail leading to Los Angeles Avenue
- 3 South side of the Arroyo Simi from the First Street to the Cascade Overlook (located between First Street and Erringer Road)

Primary segments of New Paved Trail - Type 1 alignments are located:

- 4 Existing trail leading from Tierra Rejada Road to Tierra Rejada Park, adjacent to the east side of Stargaze Place
- 5 South side of the Arroyo Simi from Madera Road to the existing bridges east of the Tierra Rejada Park
- 6 South side of the Arroyo Simi from the Cascade Overlook (located between First Street and Erringer Road) to the proposed New Paved Trail - Type 2 at Sequoia Avenue



Locations of New Paved Trail - Type 1

New Paved Trail - Type 1 shall include:

- Design trails according to Greenway Trail and Striping Standards, Sections 2.5.3, 2.5.4 and 2.5.5
- Incorporate Best Management Practices (i.e., bioswales, permeable paving, etc.) on the back side of the trail to ensure that unfiltered water does not enter the Arroyo Simi channel
- Locate trails on the existing graded dirt or gravel pathways as often as possible, but the trail may jog to accommodate overlooks/rest areas and to improve trail safety or improve visibility



View of New Paved Trails - Type 1, looking west at Rancho Simi Community Park (top) and from Madera Road (bottom)



New Paved Trail - Type 1 occurs throughout the Greenway, and there are examples of this trail type within each of the Key Study Areas. Refer to Section 2.5.2 for locations.

The Key Study Areas depicted in this section schematically describes some of the non-typical trail situations that occur on the Greenway to demonstrate how these issues might be treated. Key Study Areas shown on the following pages are:

- Cascade Overlook/Rest Area, Key Study Area 6 and 6a
- Rancho Simi Community Park, Key Study Area 7
- Sycamore Drive Bridge, Key Study Area 8 and 8a



Key Study Areas depicting New Paved Trails - Type 1

The Cascade Overlook/Rest Area incorporates a chicane to slow down trail users when approaching the bridge, and separates trail use and proposed rest area circulation with a fence.

Refer to Section 2.9c for additional information

Where feasible, separate trail and rest area circulation



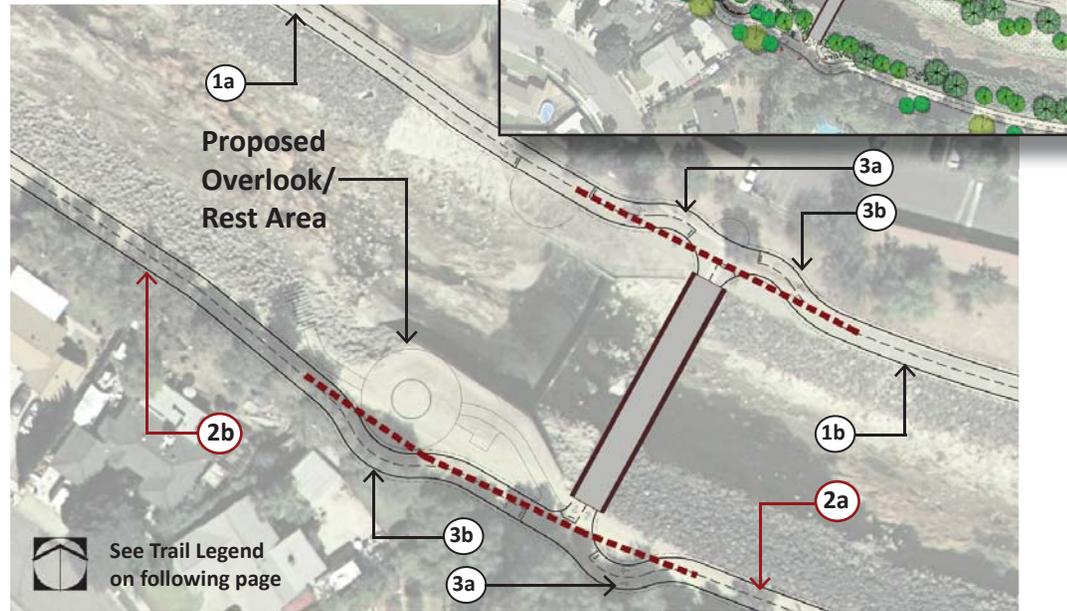
West of Sycamore Drive Bridge the trail is located behind the new overlook/rest area to allow for uninterrupted views of the existing cascade.

- Fencing at the rest area limits access to the trail
- Centerline striping stops, signage and slow bars are used to warn trail users of trail entry
- Arrows are located by the entry to immediately guide trail users

Refer to Section 2.9b for additional information

Cascade Overlook/Rest Area Trail Detail

plan view of trails



Sycamore Drive Overlook/Rest Area Trail Detail

plan view of trails

