



VISIONING STUDY

Arroyo Simi Greenway



OCTOBER 23, 2007
Final Project Report



VISIONING STUDY
Arroyo Simi Greenway

Prepared For:



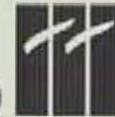
CITY OF SIMI VALLEY

and



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creating environments people enjoy

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Arroyo Simi Greenway



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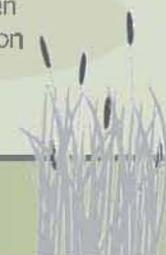
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Chapter Description

A brief overall description of the Arroyo Simi Greenway Visioning Study and Conceptual Vision Plan components, proposed enhancements and primary ideas





VISIONING STUDY
Arroyo Simi Greenway



Tuolumne River Trail

The existing Arroyo Simi Bikeway is a trail that runs through the center of Simi Valley and has an enormous potential to connect multiple parks, schools, and neighborhoods. The existing bikeway is currently an under utilized recreational and natural resource, but has the capability of becoming a valuable resource and a special place that the community is proud of and uses on a more regular basis.

The existing Arroyo Simi Bikeway is operated and managed by three agencies that include the City of Simi Valley, Rancho Simi Recreation and Park District (RSRPD), and Ventura County Watershed Protection District (VCWPD). These entities are working together to increase the use of the trail, maintain flood control requirements, and transform the Arroyo Simi into a Greenway. RRM Design Group was retained to work with staff, the Ad Hoc Committee, and the community to develop a vision for the Arroyo Simi Greenway.

The Arroyo Simi Greenway Visioning Study focuses on the scenic and recreational corridor as a whole. The Visioning Study contains descriptive recommendations and illustrative examples of possible improvements, and discusses future tasks to be completed. Included in the Visioning Study is the Conceptual Vision Plan which is a graphic depiction of the regional connections and safe routes to local parks and schools. The Vision Plan is built upon the wonderful opportunities existing along the Arroyo Simi, and developed them into a Greenway.

The scenic and recreational Greenway has the potential to increase trail use and make the Arroyo Simi a more enjoyable place to visit



The vision for the Arroyo Simi Greenway is to have an enhanced trail route which promotes more trail use, and incorporates improved access and connections with additional visitor amenities. The Greenway will serve recreational and pedestrian and bicycle commuter needs and have multiple connections to neighborhoods, businesses, schools and parks. The Greenway will increase recreational opportunities by linking the trail system to adjacent parks, and contain new areas of park expansion and overlooks/ rest areas.

Another vital component to the Arroyo Simi Greenway is natural resource protection and enhancement which creates better habitat areas while improving water quality. An important part of this enhancement is environmental education and interpretation. Interpretive exhibits are suggested along the Greenway that provides information to both adults and children about the importance of preserving habitat areas while learning about Simi Valley's rich cultural heritage.

The key to creating the Arroyo Simi Greenway is to develop a greater awareness of the trail system by improving aesthetics and creating a distinctive sense of place. Trailheads, gateways, and public art programs can be incorporated to develop a vibrancy and identity to the Greenway. Unified and consistent signage, landscaping, fencing, bridge facades, and streetscape enhancements will help to develop the Arroyo Simi into an identifiable and unique place.



A photograph of the existing Arroyo Simi Greenway between Tapo Street and Tapo Canyon Road



The Arroyo Simi Greenway already contains some habitat areas and local wildlife



Prince Memorial Greenway

The enhanced trail route with increased connections combined with improved aesthetics and habitat areas will develop the Arroyo Simi into a vibrant and unique place



Prince Memorial Greenway

The Arroyo Simi Greenway will greatly benefit the community and become a sense of pride for residents

The Visioning Study strives to change the opinion and perception of the Arroyo Simi. Through the proposed enhancements, the Greenway should become a wonderful recreational and scenic corridor that the community is involved in. The community could see increased property values based on the revitalization, and have abundant healthy recreational opportunities. With the development of the Greenway, an increased sense of community identity and pride should occur that will further spur additional involvement. The increased trail use along with trail safety improvements should increase security along the Greenway; while reducing homelessness, trash and litter problems.







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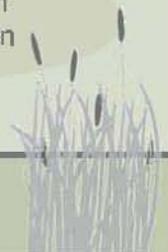
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Purpose of the Visioning Study
Project Description
The Planning Approach
Vision and Guiding Principles
What is a Greenway

INTRODUCTION
Chapter Two





VISIONING STUDY
Arroyo Simi Greenway

A. PURPOSE OF THE VISIONING STUDY

The City of Simi Valley and the Rancho Simi Recreation and Park District have a compelling vision for the enhancement of the Arroyo Simi corridor. The City's and District's expectation is to develop a plan that captures the imagination of the community, and balances the need for a plan that can be implemented in a practical and fiscally responsible fashion. The Simi Ad Hoc Committee was established to help facilitate the development of a plan, and to ensure that goals and objectives were met.

The overall goal of the Visioning Study is to identify the greenway's full development potential as a major recreational and natural resource by refocusing attention on the Arroyo Simi within the context of existing development patterns, and redefining and reinventing the image of the Arroyo. The purpose of the Arroyo Simi Visioning Study is to explore future opportunities for visual, recreational and educational enhancements along the Arroyo Simi by creating guiding principles, setting forth design ideas, and outlining a course of action.

B. PROJECT DESCRIPTION

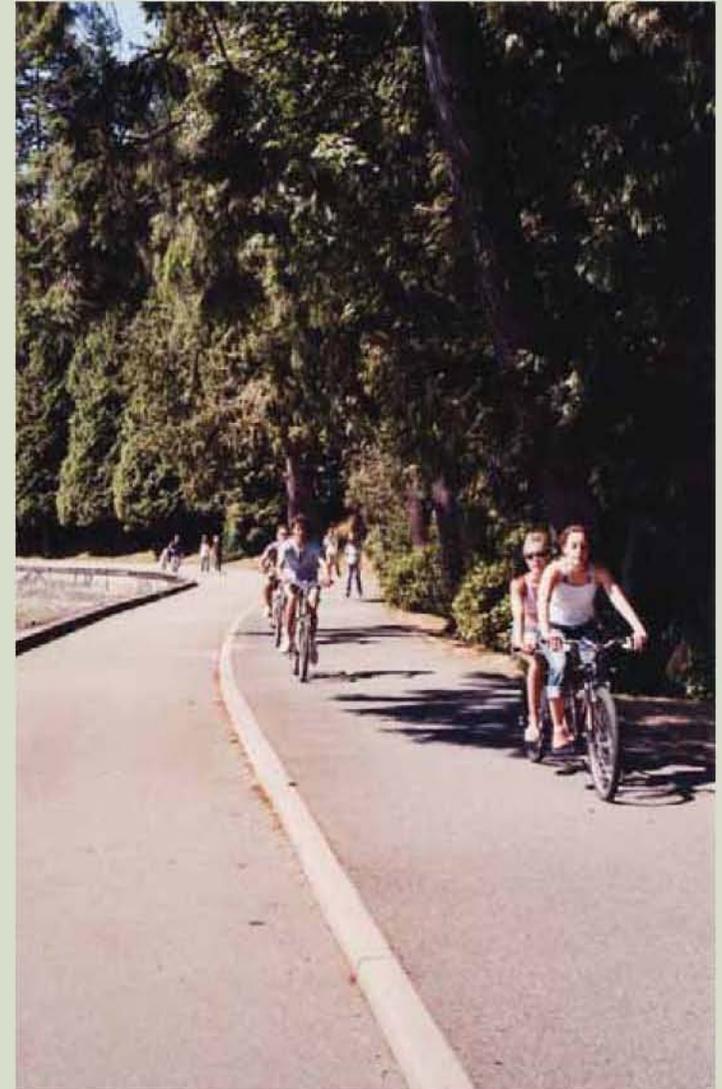
The Arroyo Simi Greenway Visioning Study covers an 11.96 mile project study area, and extends from the city limits line on the west to Corriganville Park to the east. The Arroyo Simi corridor has a great foundation: an existing bike path, considerable riparian habitat, and an impressive community park frontage of ten public parks. The Arroyo Simi Greenway has an enormous potential to become an even more valuable asset to the community and increase the frequency of use.



A decorative graphic on the left side of the page. It features a light green background with a white, wavy line representing a river or path. In the upper left, there are three stylized white birds in flight. In the lower left, there are some reeds or grasses. The overall style is clean and modern.

The Arroyo Simi Visioning Study focuses on the scenic and recreational corridor as a whole, as well as highlighting selected areas identified by the Ad Hoc Committee as being critical to the success of the project. The Visioning Study addresses the project in terms of pedestrian, equestrian, and bike circulation; while increasing recreational opportunities and connections to neighborhoods, businesses, schools and parks. The Study discusses natural resource protection and enhancement, visual resource improvements, and environmental education and interpretation. The Vision Study also outlines a strategy for maintaining the functional aspects the Arroyo Simi such as flood control and maintenance, and security and privacy.

The Arroyo Simi Visioning Study establishes the route of multi-use pedestrian and bicycle trails along the Arroyo Simi corridor. The multi-use trails are intended to promote alternative forms of transportation and provide new recreational opportunities consistent with the goals set forth in the City of Simi Valley's Bicycle Master Plan. The study contains a description and illustrative examples of conceptual improvements along the Arroyo Simi, and discusses future tasks to be completed.



Increased recreational facilities and additional connections can make the Arroyo Simi into a wonderful Greenway



A site tour provided data for discussing and analyzing the opportunities and constraints along the Arroyo Simi Greenway

C. THE PLANNING APPROACH

RRM Design Group worked with City, RSRPD, VCWPD staff and the Ad Hoc Committee members reviewing opportunities and constraints, observing specific site conditions, and developing the vision for the Arroyo Simi. The community had the opportunity to comment on the Visioning Plan and provide input for consideration by the Committee at a public workshop conducted on April 26, 2007.

Ad Hoc Committee Direction

RRM presented the Conceptual Visioning Plan to the Committee to provide the opportunity for the Ad Hoc Committee to offer input on the plan prior to the public workshop. The Committee reviewed and discussed each of the features of the plan and provided direction for refinement. The Ad Hoc Committee endorsed the Concept Plan, and supported the idea of a primary and secondary trail route and that the improvements were being dispersed throughout the entire community. The general direction was that the Concept Plan and Vision Study should be very visual and easy to comprehend.

Site Tour

RRM toured the length of the Arroyo with staff and the Ad Hoc Committee on December 12, 2006. RRM photographed and analyzed the potential for enhancing the recreational and aesthetic value of the Arroyo. The site tour provided many ideas for improvements along the Arroyo, as well as the development of guiding principles.

Bicycle Master Plan Review

RRM reviewed the City of Simi Valley Bicycle Master Plan adopted June 3, 2002 to learn about previous bicycle planning efforts. The review focused on areas along the Arroyo Simi, but also on city wide bicycle recommendations. The Bicycle Master Plan contains a detailed inventory of existing bicycle facilities and specific recommendations for the City's bicycle network. The bikeways presented in this Visioning Study are largely consistent with the City's proposed Master Plan; thus facilitating connectivity to the regional bikeway system.

Public Workshop Results

RRM Design Group facilitated a public workshop on April 26, 2007, to expose the community to the Arroyo Simi Greenway Visioning Study. The workshop included a presentation of project background, photographs of representative features and areas, interpretive sign examples, and the Conceptual Visioning Plan.

The plans received an enthusiastic reception from those attending the workshop. The community members provided comments regarding their support or concern about various concepts. Public input received was incorporated into the Visioning Study where appropriate. Public Workshop Comments are included in Appendix A of this Visioning Study.



The public workshop allowed for the local community to become involved and provide comments about the Greenway



Increased access and amenities will change the general perception of the Arroyo Simi, and will develop the corridor into a linear park that instills a sense of pride in the community

D. VISION AND GUIDING PRINCIPLES

Vision

The overall vision for the Arroyo Simi is a creation of a greenway that serves pedestrian and bicycle commuter and recreational needs, and improves air and water quality. The greenway will incorporate enhanced pedestrian and bicycle access and amenities, park expansion and habitat enhancement, and improved aesthetics.

The Greenway provides an opportunity for changing the perception of the Arroyo Simi from a channel behind the back fence of businesses and residences to a wonderful linear park with ample recreational amenities that brings in proud local residents and visitors from other areas.

Guiding Principles

During the Ad Hoc Committee discussion, a consensus developed which outlined several guiding principles that the Committee believed to be important factors leading the development of the Vision Plan. The following Guiding Principles were developed to create a plan that is achievable, sustainable & enriches the community:

- Provide for Better Access and Connections by:
 - Realigning and extending existing path;
 - Developing trailhead areas;
 - Providing pedestrian/ bike bridges and overlooks at key locations;
 - Providing additional rest and picnic areas;
 - Connecting Greenway to schools, neighborhoods, shops and restaurants

- Improve the Aesthetics by:
 - Protecting and enhancing the creek habitat;
 - Providing consistent fencing and landscaping;
 - Shielding bad views and highlighting desirable areas;
 - Incorporating kiosks, interpretive and educational signage, and public art;
 - Improving existing bridge facades and streetscapes;
 - Creating gateways and enhance entries

- Improve Safety Perception and Stewardship by:
 - Creating better visibility and signage;
 - Developing programs to control graffiti and trash;
 - Addressing short term street crossing issues;
 - Locating lighting at key points;
 - Constructing street under crossings;
 - Maintaining Flood Control functions;
 - Providing guidelines for adjacent development;
 - Encouraging adjacent property owners to participate;
 - Creating a support group to promote the Greenway and encourage maintenance of the corridor



Protecting and enhancing the creek habitat, as well as developing a consistent landscaping and fencing program will improve the aesthetics along the Arroyo Simi Greenway



Bavelion Avenue of the Plays

The greenway corridor will link community assets, and significantly benefit the City of Simi Valley



Jennifer Street Bridge, San Luis Obispo

Health and livability in Simi Valley will be increased by the ease of alternative transportation and Greenway connections available

E. WHAT IS A GREENWAY

A greenway is a linear park or a landscaped corridor found along creeks, roads and railways. They are managed for conservation, habitat, educational, and recreation purposes. Greenways are used to link and connect community, civic, and public places.

Benefits of a Greenway

Trails and greenways meet the goals of alternative transportation and recreation, and provide multiple additional benefits to the community.

- Health
 - Greenways create healthy recreation opportunities by providing people of all ages with attractive, safe, accessible places to cycle, walk, hike, jog or skate.
 - Health benefits included through regular exercise are reduced heart disease, hypertension and cholesterol.
- Transportation and Livability
 - The ability to avoid congested streets and travel through natural areas adds to a community's "livability." Connections to the MetroLink Station and five industrial parks provide a wonderful opportunity for commuters.
- Economy/Revitalization
 - Countless communities across America have experienced an economic revitalization due in whole or in part to trails and greenways.
 - The economic benefits from trail users, visitors and vacationers provide increased sales tax revenues when they purchase equipment, meals, lodging and transportation from local retailers.

- Conservation and Environment
 - Greenways protect plant and animal species, and allow people to experience nature with minimal environmental impact.
 - Trails provide enjoyable and interesting experiences that support a variety of wildlife viewing, photography and community volunteer activities.
- Historic Preservation and Community Identity
 - Trails have become sources of community identity and pride. Trails and greenways are used to highlight and provide access to historic and cultural resources.
 - Trail users receive educational benefits by learning of the natural, historical and cultural community resources, and are ideal elementary school field trips and outdoor classrooms.



The Cloisters, Momo Bay

The Greenway can educate the community by highlighting the natural environment, and pointing out significant areas and events that occurred along the Arroyo Simi



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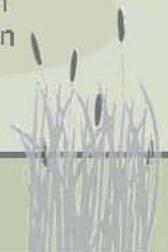
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Chapter Description

A brief history of the area
Existing types of circulation
Existing and surrounding land uses
Opportunities and constraints of the study area





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Arroyo Simi Greenway

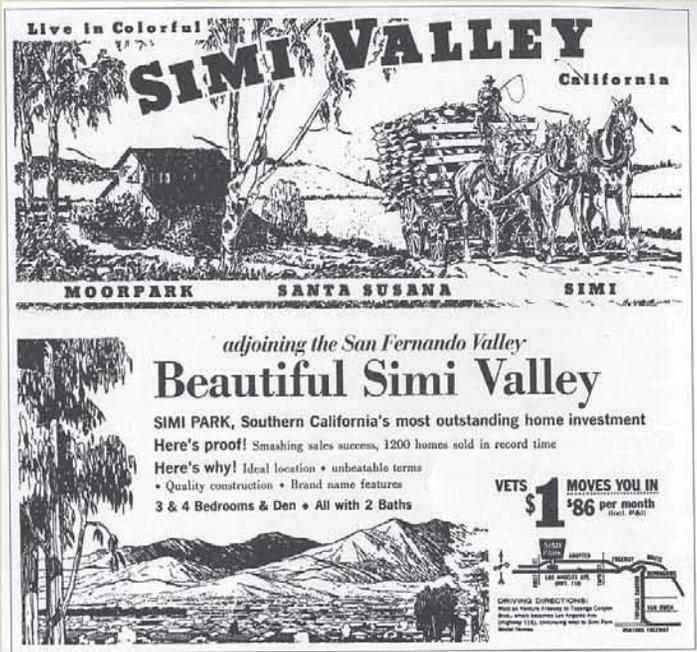


Image from "Simi Valley: A Journey Through Time" Printed with permission of author, Patricia Harens

California Mutual Benefit Society of Chicago brochure to entice settlers into Simi Valley

A. A BRIEF HISTORY OF THE AREA

The City of Simi Valley has seen considerable change, and the land around the Arroyo Simi has undergone a tremendous transformation over the years. The biggest challenge for this project is the history of land use planning and development efforts along the Arroyo Simi. Existing development has built right up to the Arroyo, but has showed little or no interest in utilizing this resource. The Conceptual Vision Plan proposes making the Arroyo Simi a significant feature which is integrated into the daily lives of residents and businesses.

Simi Valley was first inhabited by Chumash Indians who lived on what nature provided. Along the inland valleys of Simi Valley, the Indians ate seeds from sage, acorns from the oak trees, and whatever small game animals were available. Strathearn Historical Park was known as the Chumash Village, and was settled because of the Arroyo Simi and its abundant water supply.

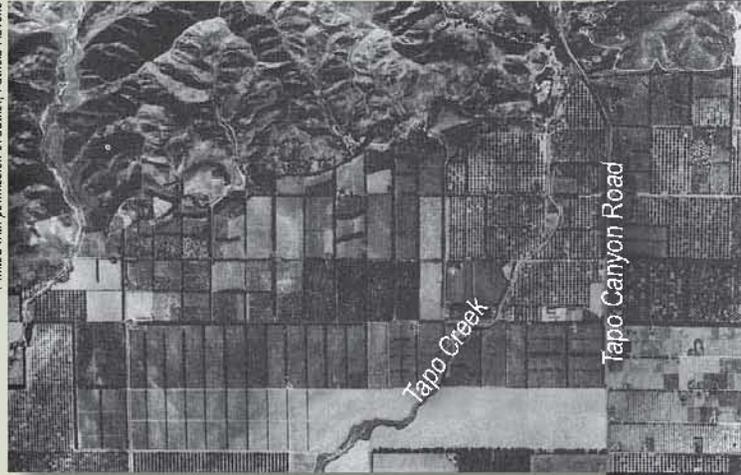
El Rancho Simi was the earliest Spanish colonial land grant within Ventura and Santa Barbara Counties, and it was one of the largest land grants ever made, approximately 113,000 acres. Simi Valley was a stopover place between the Missions San Fernando Rey and San Buenaventura, and in later years there was a stage stop nearby as well. Strathearn Historical Park still contains an adobe structure used for these stopovers.

1960
Aerial Photograph



Image from "Simi Valley, A Journey Through Time"
Printed with permission of author, Patricia Havens

Image from "Simi Valley, A Journey Through Time"
Printed with permission of author, Patricia Havens



1945
Aerial Photograph

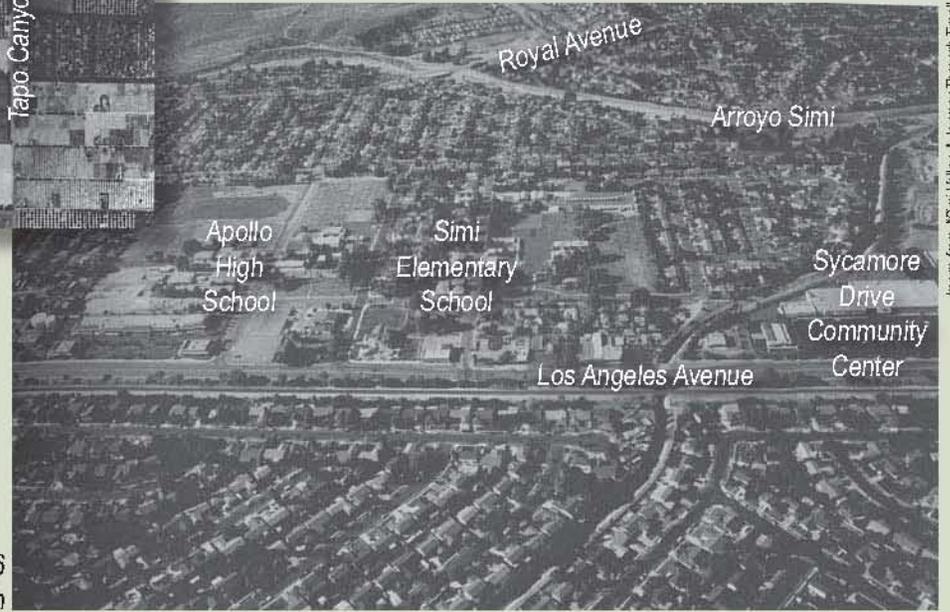


Image from "Simi Valley, A Journey Through Time"
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1996
Aerial Photograph



Image from "Simi Valley, A Journey Through Time"
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Santa Susana Airport was located along the Arroyo Simi from 1944 to the mid 1970's. An auto wrecking yard and community dump were located adjacent to the airport (note smoke in photo)

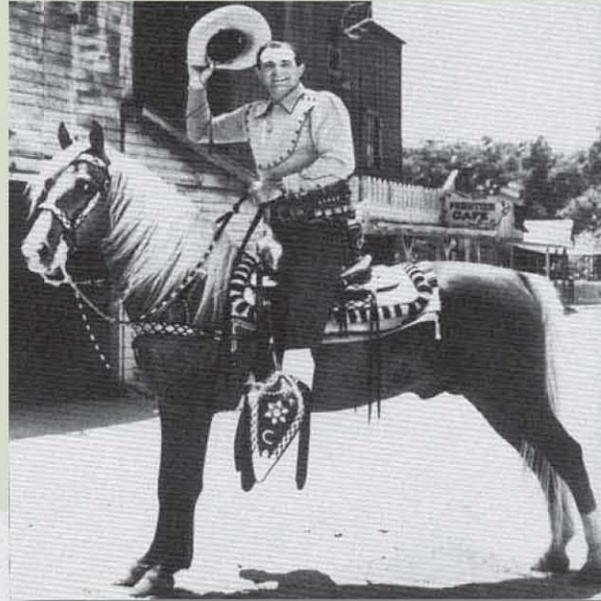


Image from "Simi Valley, A Journey Through Time"
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Corriganville was a 2,000 acre movie ranch from 1937 to 1965, and has a rich and vibrant history



Image from "Simi Valley, A Journey Through Time"
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In the 1860's and 1870's, easterners began to look around Southern California for land to farm. In the late 1880's the real settlement began when Simi Land and Water Company was formed to sell land in the first real estate promotion. Southern Pacific Railroad built a tunnel in 1904 through the Santa Susana Mountains. A depot was completed at Tapo Street and Los Angeles Avenue, and the town of Santa Susana rapidly became a center for agriculture and railroad activity.

B. EXISTING TYPES OF CIRCULATION

Today the existing Arroyo Simi Bikeway is currently under utilized with a few bike riders or pedestrians using the trail for recreation or commuting. Although equestrian activity currently occurs at the Arroyo Simi Equestrian Center adjacent to the Arroyo, very few equestrians have been observed using the existing Arroyo Simi trail system and equestrian use has developed in other areas of

town such as the Bridal Path neighborhood. New residential development along the Arroyo has not been oriented for equestrian use. Regional circulation is divided by busy roadways and multiple unsignalized crossings. The railroad and 118 Freeway north of the Arroyo create significant barriers to connecting the northern portion of the city to the Greenway. A regional trail system exists to the east and west of the Arroyo; however, it is separated from the existing Arroyo Simi Bikeway.

C. EXISTING SURROUNDING LAND USES

The Arroyo Simi is bordered by a variety of land uses, and offers an opportunity to connect multiple neighborhoods and businesses.

- From the westerly city limits to Madera Road, there are large properties that are primarily industrial, public facilities and open space.





The existing under crossings, such as this example at Tapo Street, are wonderful components already contributing to the Greenway

- From Madera Road to Erringer Road, the land uses to the north are primarily industrial and commercial with some multi-family residential interspersed between First and Erringer, and contains mostly residential uses to the south.
- From Erringer Road to Sequoia Avenue, residential land uses primarily exist on both sides of the Arroyo with a few parks and public facilities on the south side.
- From Sequoia Avenue to Stearns Street the land uses are mostly industrial and commercial on the north, with parks and open space primarily on the south. From Stearns Street to Kuehner Drive, the land uses are primarily residential on both sides of the Arroyo, and includes several schools and parks.

D. OPPORTUNITIES AND CONSTRAINTS OF THE STUDY AREA

Opportunities

Opportunities of the study area include regional trail systems, existing under crossings, existing parks, vacant lands, large drainage channels, and the potential for on-street bike lanes. There is an extensive regional trail system surrounding the Arroyo Simi, and the potential to connect it with the Greenway. This regional trail system consists of the Rocky Peak Trail, Hummingbird Trail, and Chumash Trail to the east; Rim of the Valley Trail in the center; and the Mt. McCoy Trail to the east. There are three bridges under crossings along the Arroyo Simi, and possibility in several locations to add new under crossings.

There is a wonderful park system in the City with the potential to connect many of them to the Arroyo Simi. Some of the parks have large enough facilities and parking lots to create trailheads and share uses with the Arroyo Simi Greenway. There are some vacant lands along the Arroyo Simi that provide opportunity to develop additional park land, overlooks, and rest areas. The Arroyo Simi has numerous drainage channels that are wide enough to include a trail, with many adjacent to schools and parks. Roadways along the Arroyo are wide enough to incorporate future Class II or III bike lanes and further strengthen potential regional connections (see Chapter 4 for a description of bikeway class system).

Constraints

Constraints include the lack of parking for trail users to unload bikes to use the trail, and few ADA accessible parking locations. The bikeway is in need of resurfacing and repainting. Weeds and trash are found along the path which creates nuisances for trail users. Within the Arroyo Simi, dumping and transient camps have created safety and visual problems. Trash and dumping, and grading and spraying of weeds along the Arroyo has significantly damaged stream habitat.



Multiple parks are located adjacent to the Arroyo Simi and they significantly increase recreational uses along the Greenway



Trash and dumping are currently deteriorating the overall atmosphere of the Arroyo Simi Greenway



Unsignalized traffic and railroad crossings are difficult, and bikeway entrances uninviting and hidden. The current entries to the Arroyo Simi are a significant deterrent to trail users. The entries are seen from the roadway as being gated and locked. A closer examination reveals a chicane (a tight serpentine curve at the entry) off to the side for bicycle and pedestrian use. The chicanes are narrow and tight for a bicyclist, and users are forced to get off bikes to enter the trail.

The existing trail entries, like this example west of the Amtrak Station, appear gated from the roadway and are very difficult to navigate and frustrating for trail users





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Chapter Description

A brief description of the Conceptual Vision Plan
Discussion of elements included in the Conceptual Vision Plan
Explanation of Arroyo Simi Greenway Trail System
Map of Overall Vision Plan



VISIONING STUDY
Arroyo Simi Greenway

The Arroyo Simi Greenway Visioning Study extends from the city limits line on the west to Corriganville Park to the east. The Conceptual Vision Plan looked closely at each area along the Arroyo Simi in detail, and focused on increasing trail use and making the Arroyo Simi a nicer place. Pedestrian amenities and important connections were critical to developing a plan for this potentially scenic recreational corridor. The Vision Plan studied regional locations of existing parks and schools and made efforts to link them into the Greenway trail system. The Vision Plan paid close attention to maintaining the existing uses of the Arroyo such as flood control and maintenance, while preserving security and privacy of adjacent residences and businesses. The Plan incorporated the wonderful amenities already existing along the Arroyo Simi, and built on them to develop the Greenway concept. This chapter is just an overview of the Conceptual Vision Plan; see Appendix C for additional information.

The Conceptual Vision Plan takes the Greenway elements discussed in the following chapter, and physically locates them on a plan. The Vision Plan shows the proposed locations for:

- Trails and bikeways;
- Bridges - proposed and existing;
- Parks- proposed and existing;
- Overlooks/Rest Areas - proposed and existing;
- Trailheads and gateways;
- Interpretive exhibits;
- Landscape screening and enhancement areas;
- High priority habitat restoration areas



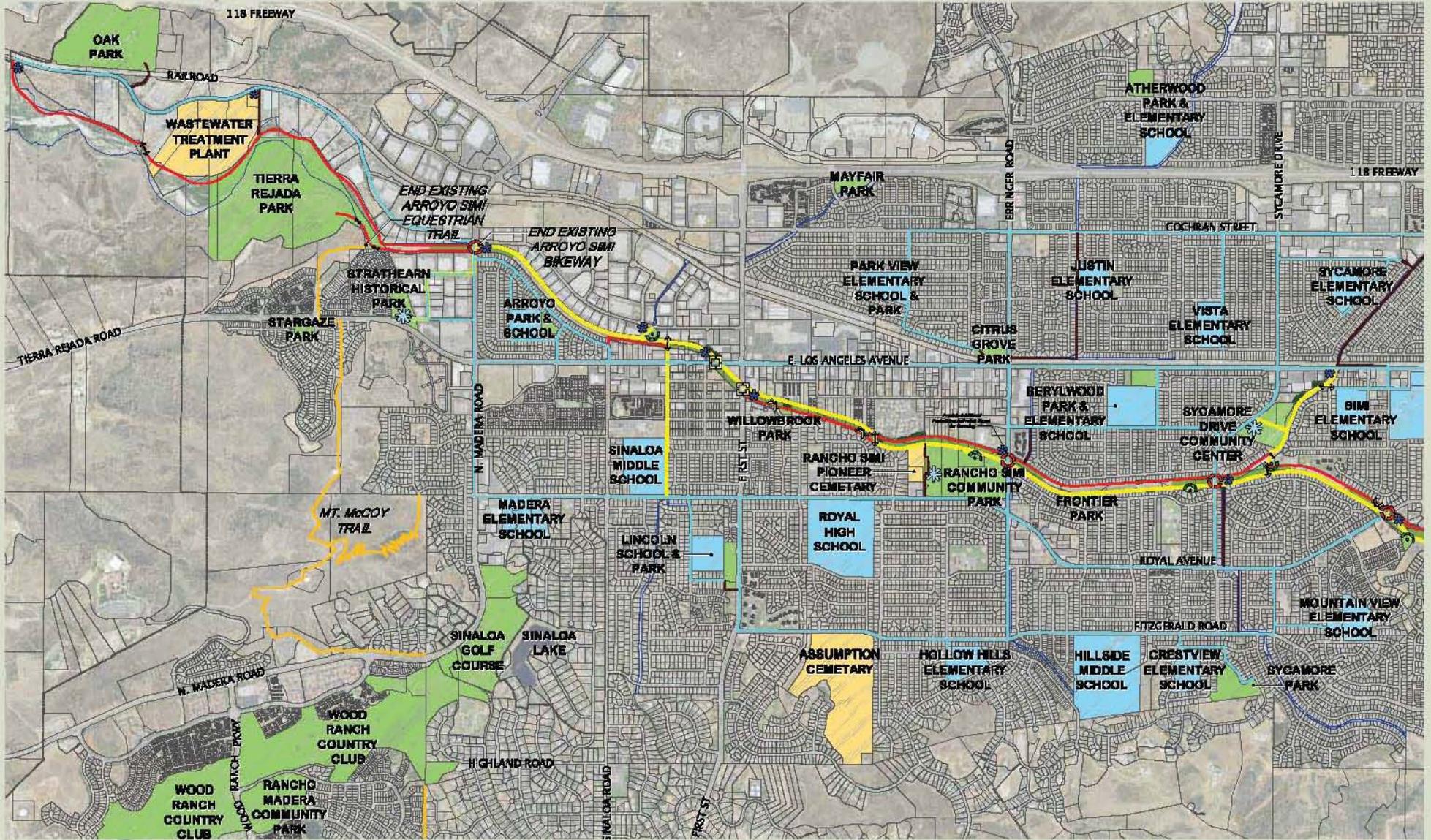
The Conceptual Vision Plan created four main types of trails for the Greenway: Primary Trail, Secondary Trail, Connector Trail and Class II or III Bike Lanes. Below is a description of each:

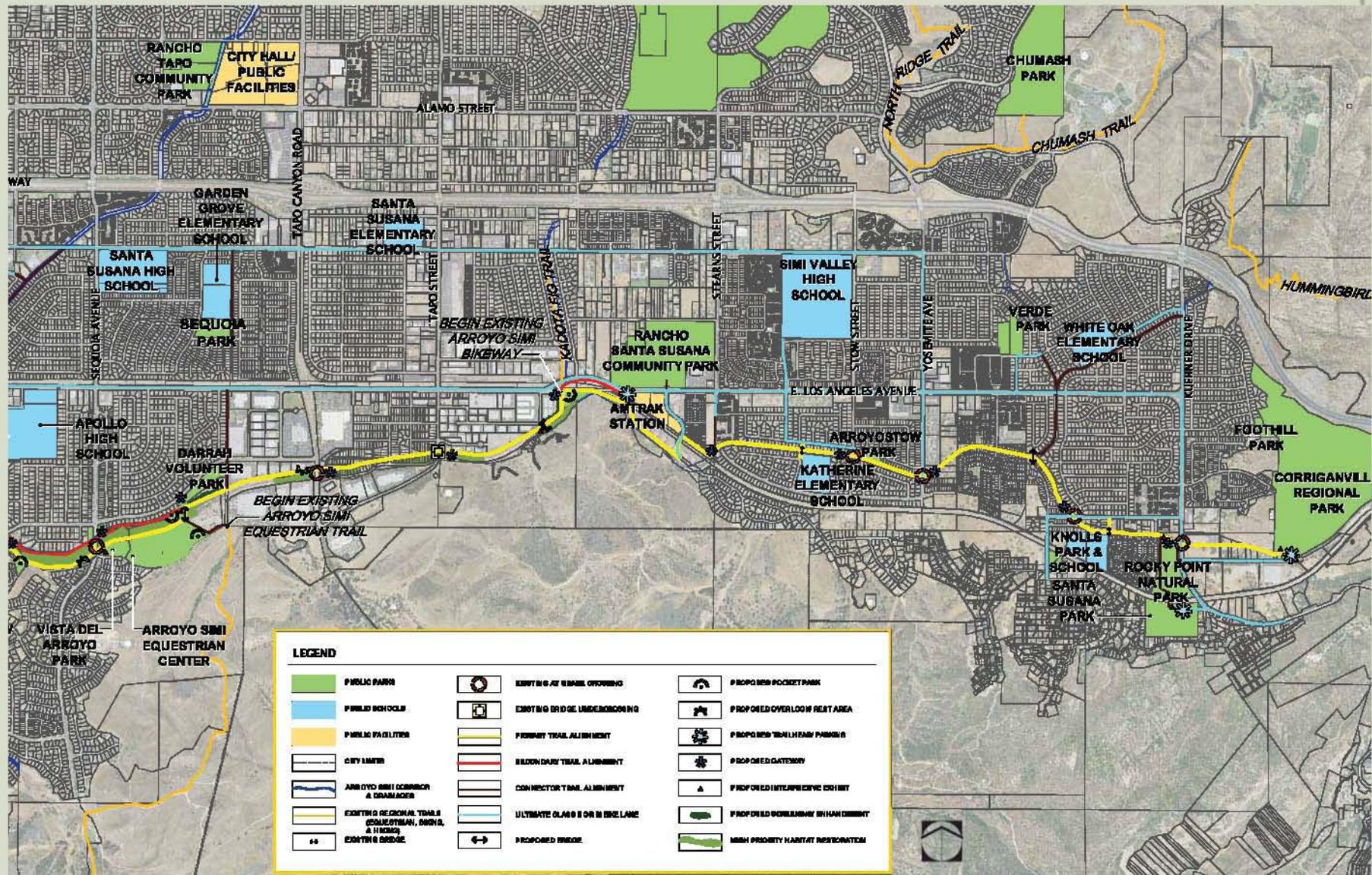
- Primary Trail – This route is envisioned to become the most used trail along the Greenway because of its wonderful connection potential and ease of travel.
 - Located on the north side of the Arroyo from the east side of Madera Road until the proposed bridge between First Street and Erringer Road. This area does not have the connectivity potential on the south side that necessitates two trails; however, a bridge is recommended which links the north side to a trail along the drainage channel on 5th Street to provide a safe route to Sinaloa Middle School.
 - Located on the south side of the Arroyo from the proposed bridge crossing between First Street and Erringer Road until the proposed crossing just south of Darrah Volunteer Park. This is a proposed new paved trail, and would be in addition to a Secondary Trail on the north side. It is proposed because of its ease of access to three wonderful parks existing directly adjacent to the Greenway, and the potential for additional pocket parks and overlooks/rest areas.
 - The Primary Trail is also proposed along the existing trail route east of the Sycamore Drive Community Center because of its link to East Los Angeles Avenue, and with the addition of a trail through the Community Center provides a wonderful connection to Sycamore Drive and Berylwood Elementary School and Park. With the addition of a bridge off School Street, this trail can link to Simi Elementary School and Apollo High School. The School Street connection also provides a potential to link to another existing drainage east of Sycamore Drive that extends up to Cochran Street, and connects to Sycamore Elementary School.
 - The Primary Trail is located on the north side of the Arroyo from the proposed bridge just south of Darrah Volunteer Park to Corriganville Park because it is anticipated to be a highly traveled area due to its connectivity potential to surrounding neighborhoods, schools, shopping, and off street convenience.





An enlargement of the Greenway shows the Primary Trail in yellow, and the Secondary Trail in red (Refer to the following pages for a map of the extents of the Arroyo Simi Greenway). The Primary Trail is a new connection on the south side of the Arroyo Simi which links the adjacent parks to the Greenway, and is envisioned to become the most traveled route because of its links to adjacent parks.





The Overall Conceptual Vision Plan shows the entire Arroyo Simi Greenway and all the proposed improvements along the corridor (Refer to Appendix C for detailed maps and descriptions)

- Secondary Trail – This trail is an important element in the design that is seen as being the less traveled side of the Arroyo Simi which will allow trail users to easily access and navigate through the Greenway.
 - The Secondary Trail is a new trail alignment located through the segment west of Madera Road to the Moorpark city limits line because it does not have the connections to places in Simi Valley, but will be important to people wishing to connect to Oak Park or Tierra Rejada Park and those needing to travel into Moorpark.
 - The Secondary Trail is also the existing trail that links the north side of the Arroyo from the proposed bridge crossing between First Street and Erringer Road, and remains on the south side until the proposed crossing just south of Darrah Volunteer Park. There is a Secondary Trail connection to the regional Rim of the Valley Trail at this proposed bridge for trail users wishing to explore further.
 - The existing sidewalk trail along East Los Angeles between the Amtrak/ Metrolink Station and Angus Avenue also is seen as becoming a Secondary Trail. It is an important link to the existing Kadota Fig Trail, Rancho Susana Community Park, and the Amtrak Station; however is not envisioned to be heavily traveled due to additional off street connections available along the Greenway.



The proposed Secondary Trail west of Madera Road to the city limits line could be partially located along the existing maintenance road



The existing sidewalk trail on Los Angeles Avenue north of the Amtrak/ Metrolink Station will become a Secondary Trail because of the addition of Greenway trail options



Existing drainage tributaries, such as this one at 5th Street, offer the potential for Connection Trails linking surrounding areas

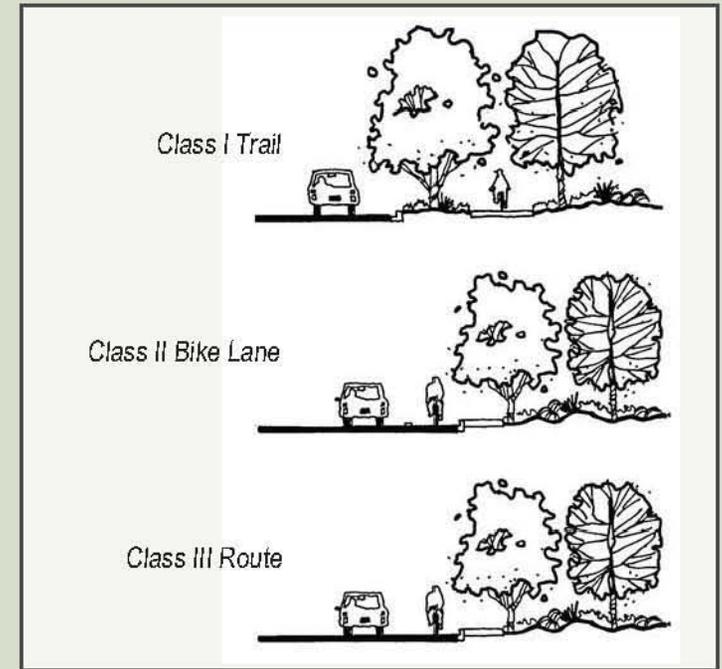


Connection Trails through existing parks, such as Rancho Simi Community Park, allow safe and easy access to the Greenway

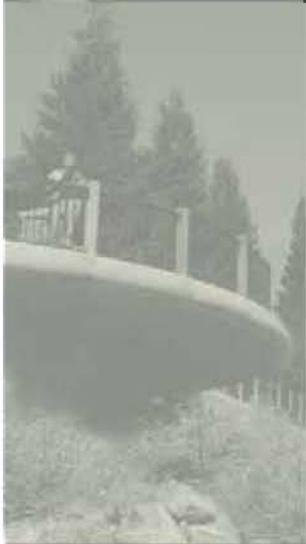
- Connector Trail - The Connection Trails do not run along the Greenway but instead feed into it, and they are considered vital to the success of future Greenway use. The Connection Trails are separated from automobile traffic and allow safe and easy access into the Greenway.
 - Connector Trails are primarily located along existing drainage tributaries which provide linkages to schools, parks, shopping, and surrounding neighborhoods. See Appendix C for additional information. Connector Trails along drainages are recommended:
 - East of Erringer Road, north and south of Los Angeles Avenue;
 - West of Sycamore Drive, north and south of Los Angeles Avenue;
 - East of Edgewood Drive and west of Kuehner Drive
 - These trails are also located through Rancho Simi Community Park, Rancho Susana Community Park, Rocky Point Natural Park, and Lincoln Park, to link Class II or III bike lanes and significant surrounding parks to the greenway system.
 - Connector Trails are also proposed along a power line easement west of Tapo Canyon Road, both north and south of Los Angeles Avenue, to link Class II or III bike lanes, Garden Grove Elementary School and Sequoia Park to the greenway system.



- Class II or III Bike Lanes - Similar to the Connection Trails, Class II or III bike lanes are vital to the trail connectivity throughout the City of Simi Valley. The trails listed above are proposed to be Class 1 trails which are separated from motorists by a space or physical barrier. Although Class I trails are preferred, Class II and III Bike Lanes are necessary for an integrated bicycle route network which connects key destinations throughout the City.
 - Class II bike lanes are located on a portion of a roadway or shoulder and separated from traffic lanes by the use of a solid white stripe on the pavement and designated for preferential use by bicyclists by signs and pavement marking. Bike lanes are typically located along collector and arterial roadways that provide direct connections through the city street system.
 - A Class III bike route does not have a designated bike lane, but is designated by signs on roadways describing it as a route for cyclists and urging motorists to share the road. Class III routes are only advisable on high demand corridors with low traffic volumes where space does not permit separated bike lanes.
 - With the addition of Class II and III bike lanes, the Greenway has the potential to link to 21 schools and 20 parks in the community.
 - Class II or III Bike Lanes are located on roadways identified in the Bicycle Master Plan, and also located on roadways which connect to schools and parks. See Appendix B for additional information.



Existing Class II bike lane on Tapo Canyon Road already connects into the Arroyo Simi Greenway trail system



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Executive Summary

Chapter Two
Introduction

Chapter Three
Project Study Area

Chapter Four
Conceptual Vision Plan

*Chapter Five
Transformation*

Chapter Six
Stewardship

Chapter Seven
Implementation

Chapter Description

Pedestrian and Bicycle Features and Improved Access
Regional and Local Connections
Park Expansion
Improved Aesthetics and Sense of Place
Protection and Enhancement of Resources





VISIONING STUDY
Arroyo Simi Greenway

A. GREENWAY FEATURES & CONNECTIONS

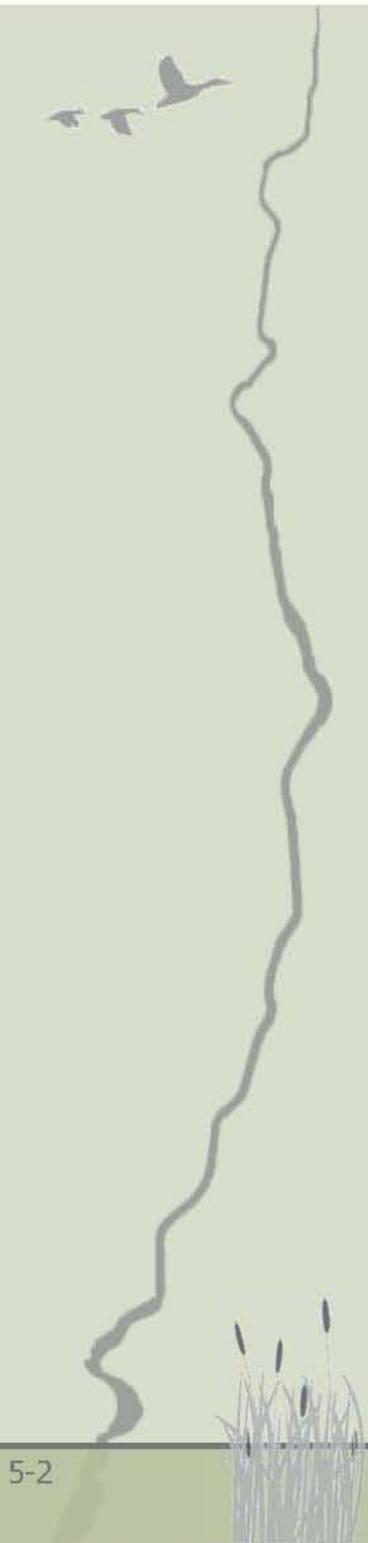
The Arroyo Simi Greenway will create a better environment for trail users, and encourage recreational activities and local commuting.

Pedestrian and Bicycle Features

- **Enhanced Trail Route**
Ease of circulation away from vehicular travel is essential in the development of the greenway. The Conceptual Vision Plan has attempted to maximize Class I bikeways whenever feasible; however in some cases, Class II and III routes were necessary.

The Conceptual Vision Plan studied the possibility to connect trail users and residents to the many wonderful parks that exist adjacent to and nearby the Arroyo Simi. This connection provides destinations that are easy to access, and significantly enlarges the greenway's recreational opportunities. The Conceptual Vision Plan linked the greenway to the parks through the incorporation of a primary multi-use trail on the south side of the Arroyo Simi from a new proposed bridge east of First Street to Darrah Volunteer Park. The trail on the north side will still remain; however, it will become a secondary route.





The two trails could become separate uses with bicycle on one side and pedestrian/ equestrian uses on the other; however additional studies are necessary to determine the feasibility of this approach. The reality of trails is that they will be used by a wide variety of users including pedestrians, joggers, in-line skaters, fitness walkers, and people with dogs or strollers. People will most likely travel in the most direct direction, regardless of any traffic control devices that say otherwise.

The proposed trail will be constructed as an asphalt or concrete trail, and be built to provide access for people with disabilities and support maintenance and emergency vehicles. Concrete was mentioned in the public workshop as a more durable material for the trail; however, it would be a much more expensive option. The bike trail could become concrete, and the pedestrian use areas remain asphalt to conserve costs and differentiate between the two uses. Equestrian areas will be concentrated near equestrian residential properties and remain unpaved. The trail will incorporate accepted design standards governing the design and construction of the trail including the Uniform Building Code, Caltrans Design Manual, and AASHTO Guide for the Development of Bicycle Facilities.

The Arroyo Simi Greenway could use existing drainage corridors to connect neighborhoods and allow for safe travel away from public roadways. Currently, people are jumping the existing fences and using these routes illegally because of their direct connections to shopping, schools and parks. The proposed enhancements would make these corridors safer to use. The drainage corridor trails will need to preserve Ventura County Watershed Protection District (VCWPD) flood protection functions, avoid operation and maintenance conflicts, and do not increase operation costs to the District. In the Conceptual Vision Plan, potential Class I trails that follow drainage corridors are shown in six locations; see Appendix C for additional information.

Even the most extensive trail network cannot provide access to all the origins and destinations in the community, and trail users have to be able to get to and from the trail on the regular street network. Class II and III bikeways will supplement the trail connectivity and will increase the use of the greenway. See Appendix B for a list of potential roadways with Class II or III bike lane additions.



BEFORE - Drainage corridor along 5th Street has a direct connection to Sinaloa Middle School from the Arroyo Simi, and many people were observed using the roadway and jumping the existing fence to walk along the corridor



AFTER - The addition of a retaining wall and sidewalk allows pedestrians and bikes to safely travel along the drainage. Decorative fencing, tree grates and landscaping enhance this trail connection.

- **Equestrian Access and Use**
Equestrian use on the Arroyo Simi appears to have declined over the years. Equestrian access will be maintained in those areas adjacent to horse properties (such as the area between the Arroyo Simi Equestrian Center and the existing Parker Ranch Trail, and the residential neighborhood located west of Sequoia Avenue and Royal Avenue), and will follow existing standards for equestrian/ multi-use trails. Potential additional equestrian areas could be located in Tierra Rejada Park and adjacent to the existing Arroyo Simi Equestrian Center. These areas are located in close proximity to regional trail systems that are more likely to have equestrian use.

All equestrian trails must consider the Ventura County Watershed Protection District's operation and maintenance functions, and make use of the best management practices for water quality.

- **Signage Enhancements**
The Arroyo Simi Greenway should incorporate signage to welcome and direct trail users. Appropriate signage can set a sense of place for the greenway, and develop a heightened awareness of the greenway to people passing by. Gateways, directional signage and mile markers are recommended signage additions along the Greenway. Interpretive exhibits are also suggested, and are described in Section C, Improved Aesthetics & Sense of Place, of this chapter.

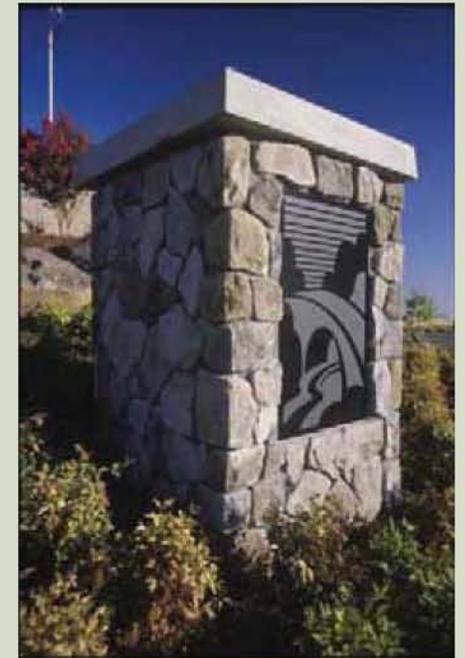
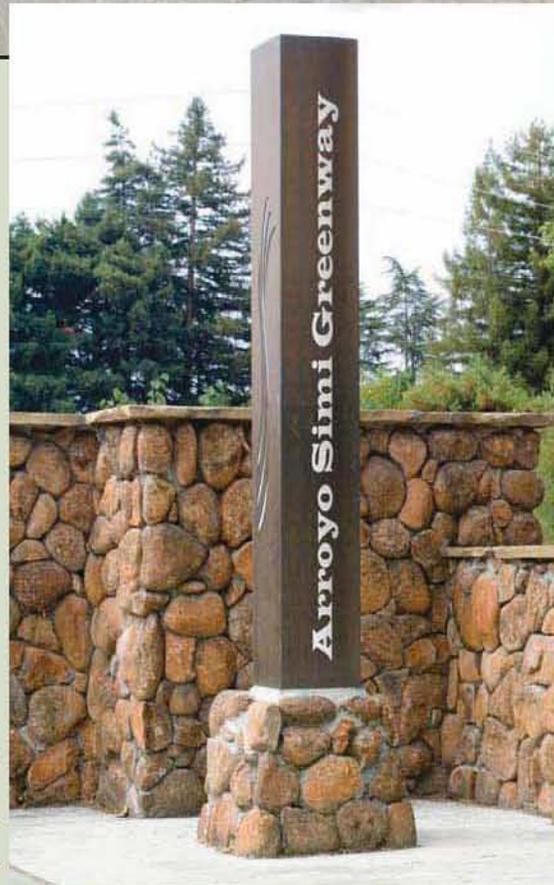


Standards should be developed for equestrian use areas, and horses need to be separated from pedestrian and bicycle trails

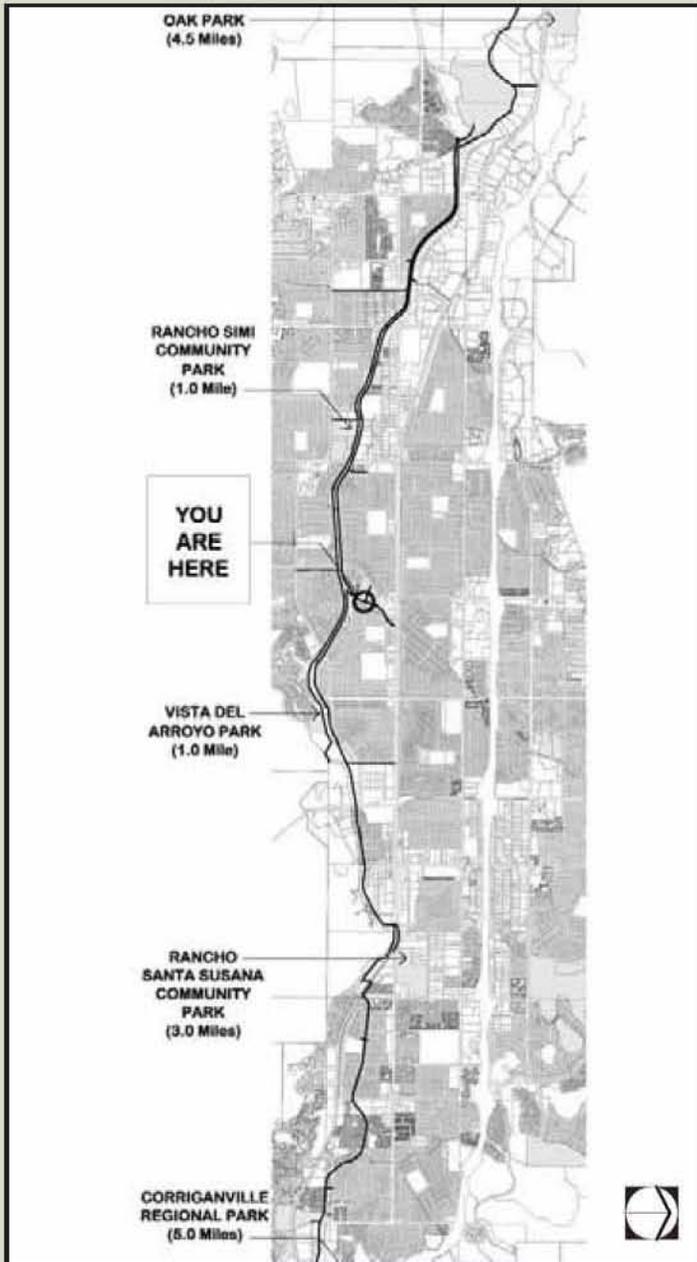


An example of a directional sign that guides trail users and is related to mile makers placed along the Greenway

- Gateways are recommended to provide a sense of entry and consistency to the greenway. Currently, there are decaying wood posts at each entry along the Arroyo Simi which originally created a sense of consistency. The proposed gateways would further enhance and build on this intent. The gateway signs should be a substantial size to be easily seen from the roadway, but not interfere with the path of travel. Two types of gateways are recommended, primary and secondary, to provide an overall sense of beginning and end to the greenway. Primary gateways are suggested at each trailhead area and on Madera Road and Kuehner Drive, and secondary gateways are suggested at all major intersections of trail and roadway.
- Directional signage is suggested to indicate length of travel to major destinations along the greenway, as well as public uses available in the surrounding areas. Additionally, symbols on the directional sign could indicate general locations of the emergency telephones. Directional signage should be located at trailhead parking areas, overlook/rest areas, parks, and along the trail in key locations.
- Mile markers should be incorporated along the greenway to allow trail users to gauge distances along the greenway, and could aid in emergency situations. The mile markers should be tied to the directional signage. Mile markers could be simple posts, small signs, or painted markings on the trail.



Some examples of gateways which would be located at major intersections along the Arroyo Simi to provide a sense of place and entry into the Greenway



Directional maps should indicate distances to primary destinations

Adequate signage and marking are essential on shared use paths. Trail users need to know about potential conflicts, regulatory information, destinations, and cross streets. Signs that clearly describe the trail condition and rules and regulations are an essential component to enhance the shared use path. Signs should be provided in an easy to understand format with limited text and graphics that are understood by all users. Written information should be in large print, and also provided in alternative formats such as Braille. In addition, simplified text and reliance on universal graphic symbols will provide information to individuals with limited reading abilities.

- **Creation of Destinations**
The creation of destinations is an important element in the development of the greenway. The intent of the destinations is to break up 12 mile trail into achievable segments, and provide potential stops along the way for people to explore. With the inclusion of trailheads, a trail user could come for a day visit, park the car and take a bike ride along the greenway to any variety of locations. The current primary destinations are Strathearn Historical Park and Museum, Rancho Simi Community Park, Vista del Arroyo Park, Darrah Volunteer Park, Rancho Santa Susana Community Park, Santa Susana Park and Train Depot, Rocky Point Natural Park and Corriganville Park. An additional future destination is recommended at Tierra Rejada Park. All of these destinations are less than three miles apart from each other.

Improved Access

- **Pedestrian and Bicycle Bridges**
New pedestrian and bicycle bridges will more conveniently and safely connect the north side of the Arroyo Simi to the south side, and link the proposed trails along existing tributary drainages. The new bridges facilitate a more attractive and functional primary trail route. The bridges should be designed in a consistent style to provide a strong sense of place for the greenway. Consideration should be given during design of the bridges regarding support of maintenance and emergency vehicles. All proposed pedestrian amenities will not interfere with channel's flood flow capacity, and should consider potential future channel improvement where possible. New bridges are suggested at seven strategic locations, and three existing bridges should be enhanced to match the style of the proposed new bridges (see Appendix C for additional information and bridge locations).



Pedestrian and bicycle bridges can link the Greenway, and create a more functional trail route





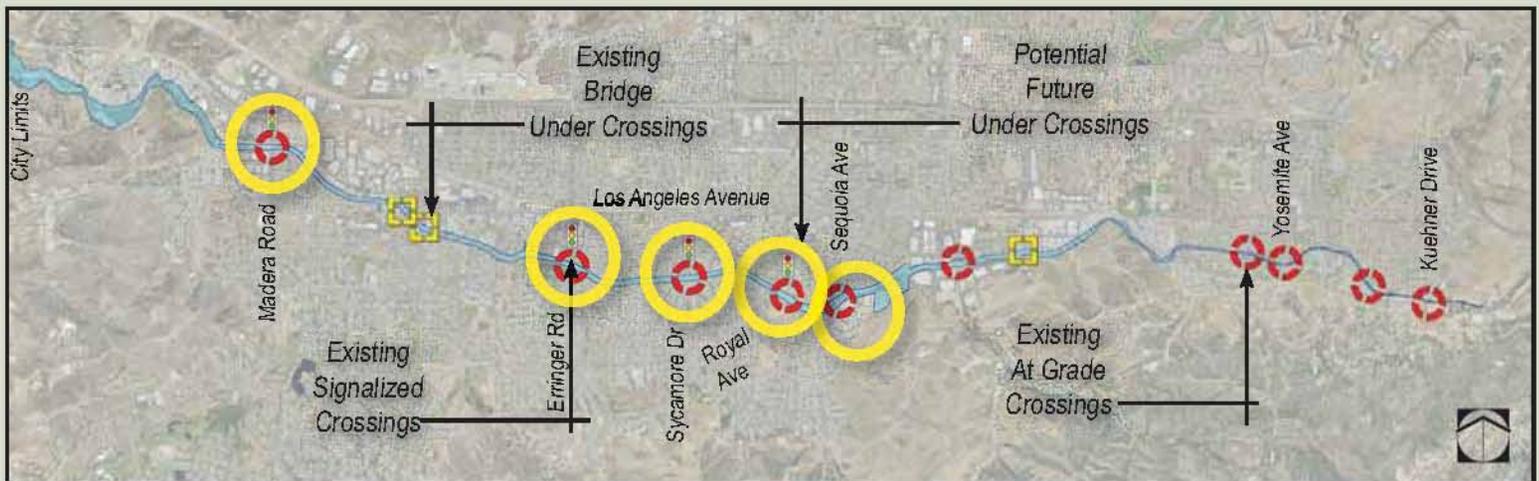
BEFORE - The existing cascades along the Arroyo Simi, such as this one located between First Street and Erringer, provide excellent opportunities for bridge crossings and pedestrian amenities



AFTER - The primary trail is located along the south side of the Arroyo, and a pedestrian/ bicycle bridge facilitates the trail connection. An overlook with interpretive signage creates a nice rest area, steps to the water and habitat enhancement educate visitors about the natural environment, and directional signs guide trail users along the Greenway

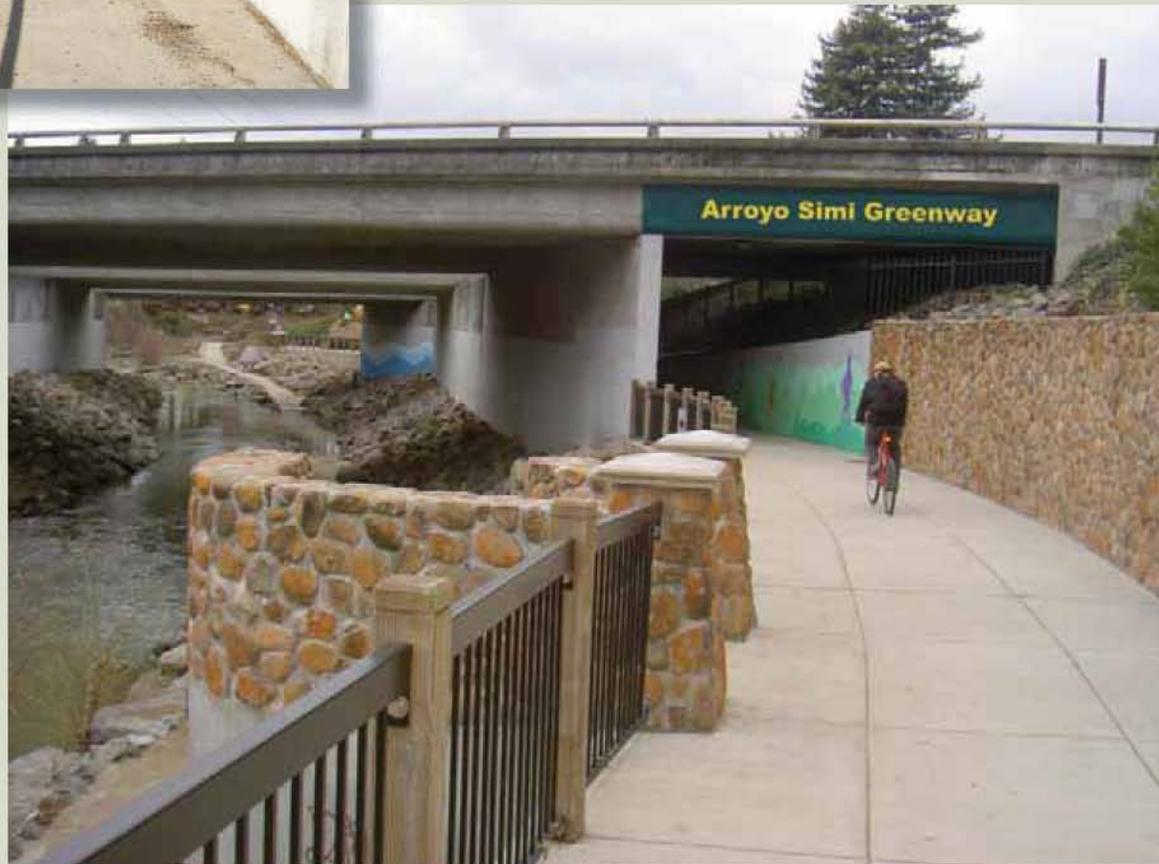
- **Bridge Under Crossings**
The Arroyo Simi has three under crossings along the trail which allow trail users to avoid potential conflicts with automobiles and reduces the need to interrupt travel along the trail. Trails that have frequent intersections with roadways require trail users to stop at every crossing, and every crossing creates potential conflicts with turning traffic. These under crossings are recommended to be enhanced with aesthetic improvements and could include consistent façade materials such as rock, bridge railing paint or restoration, decorative lighting, decorative columns and caps, and public art murals or text that emphasizes the greenway.

Five existing automobile bridges over the Arroyo Simi are tall enough to potentially allow for additional future under crossings (see Appendix C for additional information and locations). Additional under crossings would create a more user friendly trail system, and allow the Greenway to easily facilitate 5 & 10 K races by eliminating the need to close streets on race days. In the interim, as under crossings are pending, existing at grade crossings and entries will remain but be enhanced.





BEFORE - The existing under crossing, such as this example at First Street, lack pedestrian scale detail and interest and contain vast expanses of blank walls and metal railings



AFTER - The blank walls have been enhanced with rock facades and public art. Decorative fencing and steps to the water create a more attractive interface between the natural and built environment

- **Street Crossings**
The existing crossings along the Arroyo Simi are difficult in many locations. Although a majority of crossings are signalized, there are many that are not. Six unsignalized crossings are shown on the Conceptual Vision Plan which require additional study to determine the best individual crossing approach (see Appendix C for additional information and locations).

As discussed above, ease of circulation and safety are important features of the greenway development. Many traffic calming techniques are available and could be incorporated into making these crossings safer. Rumble strips, nubs, street painting, decorative crosswalks, center medians, refuge islands, smart crossings, and crossing signs are just a few suggestions.

A consistent crosswalk treatment should be chosen and used exclusively in areas adjacent to the greenway. The minimum width of 10 feet should be used due to the high volumes of anticipated use.

Nubs (bulb-outs) which extend the curb out into the street are recommended at both the intersections and in the middle of blocks to reduce pedestrian crossing distance and to improve the visibility of pedestrians and motorists. Nubs can help to slow motor vehicle traffic by physically and visually narrowing roads; however, the idea needs to be further studied by the City Traffic Engineers for safety concerns.



A consistent decorative crosswalk treatment should be incorporated into areas adjacent to the Greenway



Safe crossings are vital to the success of the Arroyo Simi Greenway, and traffic calming measures must be incorporated



San Luis Rey River Trail

Trail entries should allow for easy access for trail users and appear inviting from the roadway



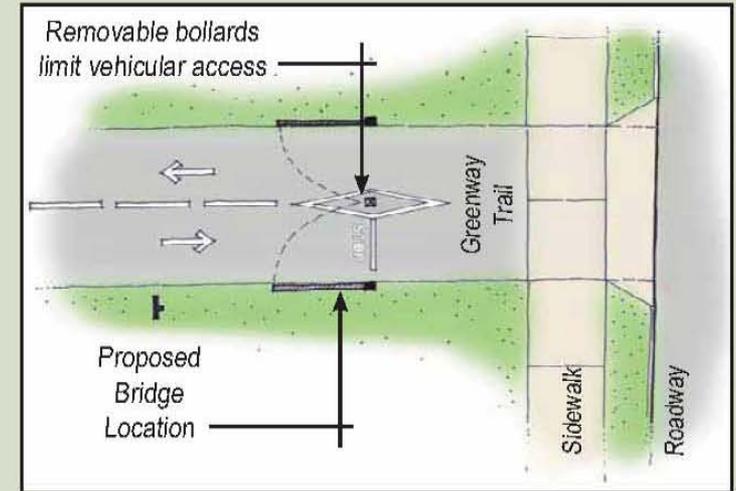
Avila Beach, Front Street

Swinging gates such as this rhino gate could allow for closure as needed, but allow Greenway visitors to easily access the trail

Center medians and pedestrian crossing islands can greatly improve pedestrian safety at street crossings with high volumes of traffic and multiple travel lanes. Medians and island breaks can reduce pedestrian crossing distances and allow pedestrians to only have to confront traffic traveling on one direction at a time. A study conducted by the Federal Highway Administration found that at pedestrian crossings without traffic signals, provision of a raised median or island had the greatest impact on reducing pedestrian/bicycle and vehicle crashes.

- **Trail Entry**
Currently the entries are focused on the Ventura County Watershed Protection District (VCWPD) maintenance vehicles, and entries for trail users appear to be secondary. The entries appear gated and locked, and tight chicanes force bicyclists off their bikes. The proposed entries to the greenway consist of lockable removable bollards and swinging rhino gates, and will be accented with landscaping and gateway signage.

Removable bollards will allow bicyclists to easily enter the trail without dismounting their bikes, and create an open and inviting atmosphere. The removable bollards also prevent automobiles from entering the trail, but allow VCWPD operations and maintenance equipment and emergency vehicles to enter when necessary. The removable bollards should be set back at least 20 feet from the back of sidewalk to allow maintenance or emergency vehicles to park while unlocking the bollards. Bollards must be highly visible with reflective materials and pavement markings around them. The swinging gates are envisioned as remaining open unless a possible flood occurs, and then they could be closed and locked for additional security.



Trail entries should be redesigned and oriented to the trail user



BEFORE - Existing Greenway entries like this example at Madera Road, are void of planting and pedestrian amenities and appear locked and unwelcoming



AFTER - The addition of gateway and directional signage invites trail users into the Greenway. Removable bollards and swinging gates create a much easier navigation onto the trail system, and a more open atmosphere. Decorative fencing and lighting, as well as landscaping and boulders, highlight the Arroyo Simi Greenway from the roadway

Proposed Regional and Local Connections

- Safe Routes to Schools and Local Parks
Creating alternative routes to schools and parks which avoid busy roadways is very important in the greenway vision. The City of Simi Valley has a wonderful potential to link 21 schools and 20 parks to the Arroyo Simi trail system through Class I, II and III bikeways. These connections could allow children and families to safely commute where now they are walking and riding on highly traveled streets.
- Transit Service
Connections between the trail access points and local transit service can encourage trail use and increase bus use. Bus stops should be located adjacent to trail entry points and/ or trailheads when feasible. Additional bus shelters and/ or benches, as well as bike racks, should be located at these stops.

- Connections to Shops and Employers
There is the potential to link many areas in the City of Simi Valley, and create a bike and pedestrian friendly atmosphere. Class I, II and III bikeways could feed into the Arroyo Simi Greenway and allow a people to safely and comfortably commute to work and shopping areas where they are now driving their cars or walking and riding on highly traveled streets. Employers could provide incentives for employees to bike to work, and health and fitness activities could take place along the greenway at lunch. Businesses along the Arroyo Simi should strive to create an open and inviting atmosphere along the Greenway and include pathways linking to the Arroyo Simi trail system to create additional commuting opportunities. See Chapter 6 for additional information.

The additional trail connections and Class II and III bike lanes can link the surrounding community to the Greenway



- Ultimate Regional Loop
On a regional scale, the Arroyo Simi Greenway is a vital trail corridor within the “Ultimate Loop” of Southern California. The Ultimate Loop is a regional system of planned and constructed trails that will ultimately connect several communities in both Ventura and Los Angeles Counties to each other, major destinations, and neighboring regional trail networks. Ultimately, connections to the Coastal Trail and Pacifica Crest Trail via the Arroyo Simi Trail’s link in the Ultimate Loop would provide Simi Valley residents the ability to reach Mexico and Canada. An assortment of trail sections make up this Ultimate Loop including:
 - Simi to the Sea Trail (Simi Valley through Moorpark to the Santa Monica Mountains);
 - Saugus to the Sea Trail (Santa Clara River to the Santa Monica Mountains);
 - Backbone Trail (Santa Monica Mountains to Point Mugu);
 - Coastal Trail (Point Mugu to the Santa Clara River);
 - Santa Paula Branch Line Trail (City of Ventura through the Santa Clara River Corridor);
 - Santa Clara River Trail (Ventura County to the City of Santa Clarita)

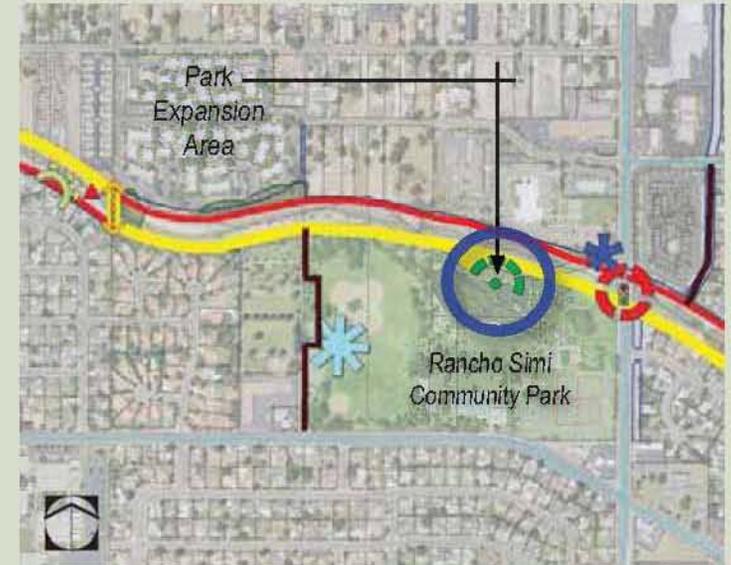


B. PARKS AND REST AREAS

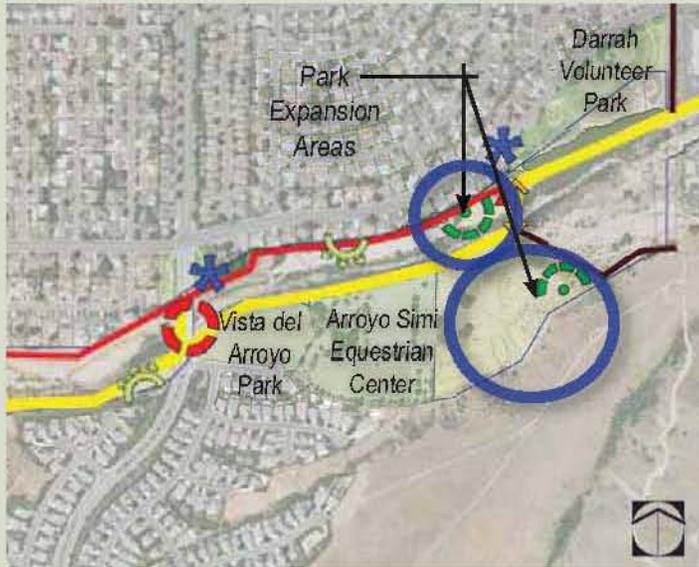
Several vacant parcels were identified along the Arroyo Simi that are within close proximity to the Greenway, and are large enough to facilitate park expansion and pocket parks. Rest areas and overlooks were located along the trail based on available space and exceptional views.

Park Expansion

- **Rancho Simi Community Park**
With the relocation of the primary trail to the south side of the Arroyo, Rancho Simi Community Park becomes a more significant part of the greenway. This park edge is currently defined by fencing along the Arroyo Simi but is envisioned to become more incorporated into the overall greenway circulation system. A trailhead is proposed in the existing southwestern parking lot, and pedestrian connections between the park and greenway should be added. Areas adjacent to the Arroyo Simi should be enhanced and incorporate pedestrian amenities that are directed towards the greenway.



Photograph of the area adjacent to Darrah Volunteer Park which has the potential for park expansion



- **Darrah Volunteer Park**
The area adjacent to the west side of the Darrah Volunteer Park has the potential for park expansion along the greenway. The park expansion area is 1.3 acres in size, and is currently being utilized as a stock pile, maintenance site, and training area for VCWPD. The park expansion area could merge the existing park and greenway, and has the potential to provide passive park amenities not currently found at Darrah Volunteer Park.
- **Adjacent to Arroyo Simi Equestrian Center**
With the relocation of the primary trail to the south side of the Arroyo, the land adjacent to the east side of the Arroyo Simi Equestrian Center becomes a wonderful potential park expansion addition to the greenway. There is a large area of 10 acres which is currently being utilized as a maintenance yard for VCWPD. The Conceptual Vision Plan proposes that this area be considered for acquisition by Rancho Simi Recreation and Park District and develop the park expansion area for either active or passive uses, as well as an equestrian trail which could connect to Corriganville Park.



The large area east of the Arroyo Simi Equestrian Center is a wonderful opportunity for park expansion

Additional Pocket Parks

Additional pocket parks should incorporate benches, picnic tables, directional signage, landscaping and turf areas, and trash and recycling receptacles. In select areas additional amenities such as parking, workout equipment, tot lots, interpretive signage, drinking fountains, restrooms, and decorative lighting could be included to further enhance these areas. Accessible designs for amenities such as benches and restrooms are critical to provide access for all users. Proposed pocket parks located adjacent to residential areas shall be carefully designed to minimize privacy and security impacts. In addition to the park expansion areas listed above, five potential locations for additional pocket parks are shown on the Conceptual Vision Plan (see Appendix C for additional information and locations).



Barney Schwartz Park, Pasco Robles

Additional pocket parks can supplement the existing public facilities adjacent to the Arroyo Simi Greenway





Rest Areas and Overlooks

Rest areas and overlooks take advantage of the larger spaces along the greenway to highlight the beauty of the Arroyo Simi. The design of the overlook areas should help to promote the new identity and sense of place for the greenway corridor. Proposed Overlooks/ Rest Areas located adjacent to residential areas shall be carefully designed to minimize privacy and security impacts. Four new pedestrian overlooks/ rest areas are proposed in the Conceptual Vision Plan, and two existing overlooks/ rest areas are recommended to be enhanced (see Appendix C for additional information and locations).



An example of an overlook that provides rest areas to enjoy the scenery and local wildlife



Rest areas and overlooks should incorporate benches, directional signage, habitat enhancement, bicycle racks, and trash and recycling receptacles. Accessible designs for benches are critical to provide access for all users. In select areas additional amenities such as workout equipment, interpretive signage, access to the water and decorative lighting could be included to further highlight these areas.

C. IMPROVED AESTHETICS & SENSE OF PLACE
The Arroyo Simi Greenway will develop into a popular linear park and refuge that connects people and places to one another. The creation of a strong sense of place for the greenway will promote a sense of accomplishment for trail users and pride for local residents and business owners, and create a better awareness and perception of the greenway.

The sense of place should be developed along the greenway to provide a sense of entry and consistency to the greenway. A few methods of developing the sense of place are creating recognizable elements that are visible from public roadways, as well as consistent and cohesive program of improved aesthetics. Below are some potential methods of creating a special sense of place along the greenway.



Prince Memorial Greenway

The Arroyo Simi Greenway can become a community refuge which has a cohesive and unified palette of improvements



Guadalupe School Lake Wetlands

Examples of interpretive exhibits depicting local wildlife and history. Exhibits could include games to appeal to children and adults





Enhance the Education Potential

A key element to the sense of place for the Arroyo Simi Greenway is education about the watershed and water quality, Native American and cultural history, and native flora and fauna. The understanding of the environment and history around the Arroyo Simi creates a unique sense of place for the greenway that will improve the reputation and use of the Arroyo Simi. The greenway could be a wonderful destination for school field trips and outdoor classrooms.

Interpretive exhibits can help to develop a sense of place for the Arroyo Simi Greenway, and at the same time educate and entertain trail users

Interpretive exhibits should be located along the Arroyo Simi Greenway in key locations which have historical, cultural and/ or natural significance



This educational component could occur in the following forms:

- Interpretive exhibits and kiosks - placed at strategic locations to highlight important areas and events along the Arroyo Simi;
- Native plant education – signs that call out native plant names will help people recognize native plant material. Additional signage could describe Native American uses of native plants;
- Public Art - discussed below

Public Art Opportunities

Local and regional artists can further contribute to the greenway's sense of place by developing public art that fits the community's environmental and historical identity. The public art could rotate to allow for the art to be dynamic and changing and create renewed interest for trail users. Permanent installations of public art could also be well integrated into the greenway design. An example of this kind of art would be similar to the existing artwork in Vista del Arroyo Park. Additionally, consider allocating spaces for graffiti art to allow for creative expression without diminishing the character of the area. Specific areas for public art have not been determined and will be selected based on art proposals.

Possible locations for public art include:

- Trail Entry Areas;
- Overlooks/ Rest Areas;
- Parks and Pocket Parks;
- Bridges

Streetscape Enhancements

The bridge façade improvements should be visible from the surrounding street to heighten the sense of place to passers by. Some additional improvements that could occur along around greenway entries are decorative lighting, decorative crosswalks, enhanced benches with paintings of Arroyo wildlife and scenery, signage depicting the Arroyo Simi, enhanced crossing signage, and street tree and shrub planting.



Prince Memorial Greenway



Prince Memorial Greenway

Public art and decorative lighting should be incorporated into the streetscape adjacent to the Arroyo Simi to enhance the image and awareness of the Greenway



BEFORE - Large blank concrete walls on existing bridge abutments create a dismal and cold atmosphere



Prince Memorial Greenway

AFTER - The addition of public art enlivens the space and creates an educational focal point. The decorative bridge railings on the existing guardrail and new fencing along the creek corridor adds to the sense of place along the Greenway

D. PROTECTION AND ENHANCEMENT OF RESOURCES

Habitat Restoration

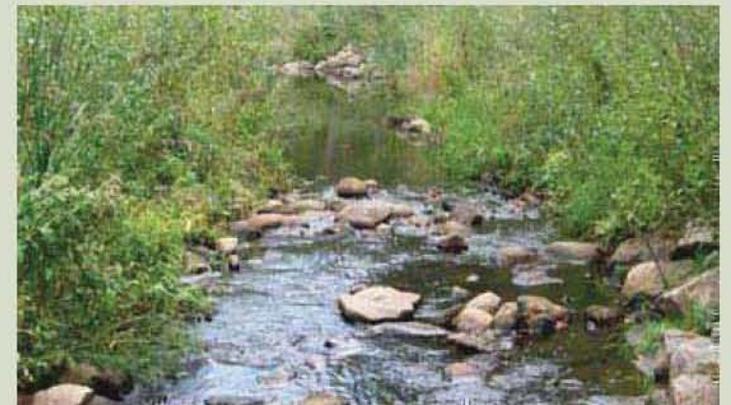
The restoration and management of the Arroyo Simi's habitat is critical in the implementation of the greenway. The natural corridor has been channelized in many locations and riprap used to control erosion. The goal is to create a more natural environment while preserving VCWPD flood control capacity and returning the Arroyo Simi to a more natural habitat area for wildlife with better water quality. All planting along the Arroyo should be carefully planned to eliminate operations and maintenance impacts to VCWPD facilities, and preserve unobstructed access.

Restoring the entire Arroyo would be ideal but due to cost constraints the Conceptual Vision Plan has focused on areas viewed from the roadways, parks, and overlook/ rest areas and substantially degraded areas as high priority habitat enhancement areas (see Appendix C for additional information and locations). Many other areas along the greenway could benefit from habitat restoration, and as funds become available additional habitat enhancement should be a long term goal.



Prince Memorial Greenway

Slope planting will reduce the visual distraction of rip-rap along the Greenway, soften the appearance of the Arroyo Simi, and develop into additional habitat areas



Habitat restoration will improve water quality while creating a more natural environment for local wildlife



There are some birds currently using the Arroyo Simi Greenway, and many are expected with enhanced habitat areas

Currently there are birds and fish using the Arroyo Simi; however, enhancement of habitat areas can significantly improve the local wildlife. Repair and enhancement of native habitats could include riparian planting and restoration with native trees and shrubs. Erosion control measures such as stabilizing banks should be added to help reduce sediment in the Arroyo.

The approach to restoring the Arroyo Simi habitat will primarily include slope bank planting. The concept is to remove pockets of existing rip-rap and install riparian vegetation. This approach has worked in other urban projects such as Prince Memorial Greenway in Santa Rosa California. As mentioned above, VCWPD will be active in reviewing and approving plans for restoration and flood capacity will be carefully maintained. The Tree Advisory Board should be consulted in as part of it's charter.





Invasive plant species, construction debris, and weedy trees in the Arroyo will be removed to improve habitat. Larger trees will be retained, and additional trees of desirable native species will be added to improve erosion control and habitat. A mix of short plants selected for the site conditions can provide additional erosion control and assist in controlling new weed growth.

After habitat restoration has occurred, it will be necessary to protect the greenway environment. Some methods of maintaining the habitats are to facilitate public education, create better regulations on dumping and cleanup, develop water quality and vegetation monitoring, and perform habitat assessments.

Landscape Screening and Enhancements

Landscape screening and enhancement areas include vegetation intended to shield poor views and create a more intimate scale for trail users. A consistent landscape palette that fits into the regional environment and creek corridor habitat should be used to add to an overall sense of place for the greenway. Many areas along the greenway could benefit from landscaping; however, locations called out on the Conceptual Vision Plan have been highlighted as specific areas seen as needing landscape screening and enhancement (see Appendix C for additional information and locations).



A consistent landscape palette which includes screening, enhancement, and riparian plants will develop a special sense of place along the Arroyo Simi Greenway



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Involving Neighboring Properties
Increasing Community Involvement
Additional Safety and Security
Shared Use Issues





VISIONING STUDY
Arroyo Simi Greenway

A. INVOLVE NEIGHBORING PROPERTIES

Though the Arroyo Simi Greenway Vision Plan focuses on public land in and adjacent to the Arroyo, changes to surrounding properties could also have a positive effect on the Greenway. Creating development incentives and restrictions to development in the area around the Arroyo Simi could help to give a more coherent look to the environment, increase transit, pedestrian and bicycle use, and create a more walkable community.

An Arroyo Simi Greenway Master Plan will be developed and it will describe possible methods for involving neighboring properties. The following concepts are some of the potential tools for reviewing new development and redevelopment proposals within the Arroyo Simi Greenway area, and should be used in conjunction with existing City policies and guidelines.

Arroyo Simi Greenway Overlay District

One potential method of managing development around the Arroyo Simi Greenway is an overlay district which should be created in conjunction with the preparation of a comprehensive land-use plan. An overlay district is an additional zoning requirement that is placed on a geographic area, but does not change the underlying zoning. The City could use the Overlay District to specify requirements and design recommendations for the Greenway in addition to standard zoning requirements.



Development Guidelines

Design Guidelines are another tool that could be used to reinforce the community's vision for properties adjacent to the Greenway. The following list is just a sample of what could be included for Design Guidelines:

- Standards for fencing, paving treatments, landscaping and lighting along the greenway;
- Methods of minimizing the obstruction of views into the greenway;
- Incentives to provide pedestrian amenities such as adequate sidewalk widths, benches, landscaping and street trees, and pedestrian oriented lighting;
- Standards for providing direct connections and signage to the greenway and surrounding transit stops and bicycle parking;
- Methods for providing secure and convenient bicycle parking;
- Standards for orienting the buildings and their entries to the street, yet creating an open atmosphere and secondary entrances on to the greenway. Attractively designed open spaces (plazas or landscaping) should be provided adjacent and potentially linked to the greenway to create opportunities for people to meet and socialize in public places. Some potential examples are:
 - Private walking trails
 - Community gardens
 - Private park areas
 - Tot lots
 - Half-court basketball
 - BBQ/picnic areas
 - Outdoor dining areas and patios



The Network, San Luis Obispo

Development should be oriented towards the Arroyo Simi and contain links into the Greenway trail system



Interested members of the community can make a big difference in the quality of revitalization of the Arroyo Simi Greenway

B. INCREASED COMMUNITY INVOLVEMENT
The key to creating a greenway that the community is proud of and will care for is to have them take part in the restoration and maintenance activities. The City should work with interested members of the public and schools to organize a grass roots effort aimed at stewardship. The organized citizens could then reach out to other people in the community to spur interest. Some potential stewardship ideas include:

- Friends of the Arroyo Simi Greenway Stewardship Group;
- Arroyo Simi Greenway Newsletters;
- Adopt a Trail Program;
- Community Cleanup Days (expand existing program);
- Volunteer Restoration Projects;
- Community Art Projects;
- Neighborhood Councils;
- Involvement of local schools



Children can become involved and learn about habitat restoration and native plants and wildlife, and see the impact they can make



Local artists can contribute their skills and/ or materials to develop the Greenway into a unique and interesting place

Prince Memorial Greenway

C. INCREASED SAFETY AND SECURITY

Crime can occur anywhere, including our trails and parks. It is important that a special effort is made to protect trail users and residents and discourage crime. The Arroyo Simi Greenway will implement a trail safety improvement program of increasing lighting, maintaining views, installing emergency telephones, and increasing surveillance.

Surveillance and Monitoring

The Arroyo Simi Greenway has been designed to be adaptable for patrol vehicles and other emergency vehicles. Ensuring the safety of trail users and residents has been a prime objective throughout the design process, and will help to guarantee a more enjoyable visit to the greenway.

A recommended addition to the safety of residents and trail users is to have surveillance along the greenway. This was mentioned at the public workshop to be especially important at lunch times, after school and in the evenings. The Police Department, Ventura County Flood Control District and/or Rancho Simi Park District Rangers could ensure that people utilizing the greenway adhere to the rules and regulations and interact with trail users. The availability of Police Patrol Officers to enforce the rules and regulations of the Arroyo Simi Greenway will be determined by staffing availability, service priorities and call volumes. An additional recommendation is to have the Department's Citizens on Patrol Program volunteers trained by local law enforcement officers help to patrol the Arroyo Simi.

Safety Precautions

The increased use and design of the greenway will significantly reduce potential safety hazards, and create a safer environment for trail users and residents. Some design elements to further reduce safety hazards are:

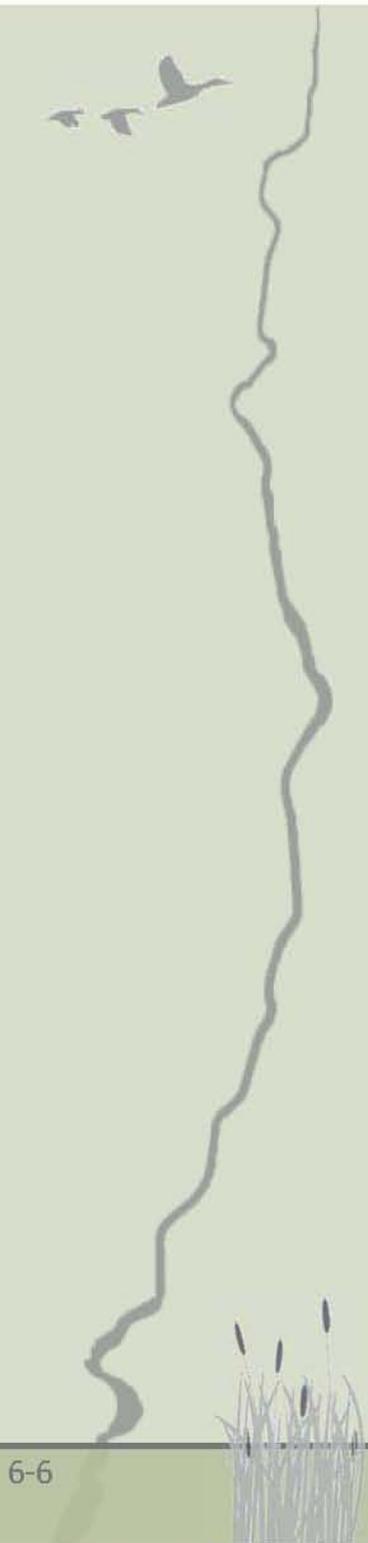
- The views must be open into and through the greenway, so that most areas are visible from the street. Plantings should be kept low to maintain good visibility, and the dense under story of invasive plants should be removed.
- Emergency telephones should be considered along the greenway and in parks that permit emergency calls at no cost (911 calls only). Mile markers should be installed along the trail to aid callers in identifying their location.
- Decorative lighting in key locations should be provided to allow for views into the greenway at night, yet not provide glare into adjacent residential areas. Lighting should be used to improve visibility along the trails and at intersections, and is critical for underpasses. Lighting will help to reduce dumping, illegal activities, and transient camps.
- Provide signs that describe potential conflict areas and list the rules and regulations of the shared use path.

D. SHARED USE ISSUES

Shared use paths provide access for a variety of users to many valuable benefits including transportation links, shopping centers, recreation areas, and habitat corridors. Shared use paths attract a variety of user groups who often have conflicting needs. There are certain design elements that are important for the successful and safe operation of a shared use pathway.

- Providing signage that clearly indicates permitted users and rules of conduct.
- Describing the path length and maximum grades that will be encountered.
- Ensuring that the shared-use path provides sufficient width and an appropriate surface for all intended trail users, or providing alternate paths for different types of users.
- If space permits, bicyclists, horses and pedestrians could have different lanes or pathways to providing sufficient separation for users traveling at different speeds.
- Considering the needs of people with disabilities within all of the user groups permitted on the path. Longer and wider equipment may need additional maneuvering space when entering the trail system, in restrooms, and when transferring from the chair to benches.





Protection of Ventura County Watershed Protection Uses

The greenway will continue to share uses with the Ventura County Watershed Protection District (VCWPD). This shared use has been working well, and this relationship is anticipated to continue in the future. The primary purpose of the Arroyo Simi is a flood control channel, and Greenway enhancements will carefully maintain existing operations and maintenance of this facility. Early coordination should occur with the VCWPD for plan consistency and integrity. Some of the issues that may require additional study and coordination with the VCWPD are:

- Maintaining current flood control capacity;
- Preserving Operation and Maintenance uses along the Arroyo;
- Ensuring enhancements do not increase operation costs to the VCWPD;
- Allowing for future flood improvement projects;
- Greenway Enhancements:
 - Trail along existing tributary drainage channels
 - Trails paved on both sides of the Arroyo Simi in some locations
 - Additional bridges and overlooks
 - Landscaping and habitat restoration
 - Additional riparian vegetation on rip-rap slopes
 - New entry gate design and necessary access clearance
 - Additional street under crossing
 - Addition of bridge façade materials



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Next Steps for Implementation
Funding Concepts

IMPLEMENTATION
Chapter Seven





VISIONING STUDY
Arroyo Simi Greenway

The Visioning Study provides the foundation and concept for the City of Simi Valley and Rancho Simi Recreation and Park District as they jointly pursue development of the Arroyo Simi Greenway. The implementation recommendations below are schematic and are meant as a starting point for further conversation and meaningful action.

A. NEXT STEPS

Prepare an Arroyo Simi Greenway Master Plan which will describe the details of what, where, when, how the greenway will be developed. The Master Plan will address the initial concepts in a much greater detail. The Master Plan is necessary to ensure a coordinated public improvement program along the Arroyo Simi Greenway. Whereas the Visioning Study initiates the creation of the greenway and outlines opportunities for creating a unique public resource, the Master Plan will develop the concept further and provide detailed design work. This Master Plan should include:

- Phasing, costs, design development, programming, and implementation measures;
- Stewardship programs to initiate;
- Environmental review and clearance (parallel effort);
- Ventura County Watershed Protection District coordination and assessment
- Public Improvement Financing Plans;
- Acquisition strategies for those portions of the Arroyo Simi where public access is not guaranteed





In addition to the Master Plan, the City Council should authorize preparation of updates to any related planning documents as necessary. This will provide consistency in the planning process and give an opportunity for public participation. Timing for the planning documents updates should be reviewed with the City Planning Department staff, and could be integral with General Plan updates.

The City and District should consider preparing a Public Participation Plan to provide an avenue for regular dialog with all stakeholders along the Arroyo Simi Greenway. This is to develop a consensus support group with common interests, benefits and objectives. Through the public participation process, a concentration on the development of state legislative support will be instrumental in future funding resources. Looking forward toward providing project funding for the greenway development, documentation of public support along with business partnerships will be essential.

A project engineer's cost estimate should be prepared and matched with appropriate funding sources for the Arroyo Simi Greenway. Staff should consider requesting City Council and the District Board to establish a budget line item for ongoing Greenway planning efforts including implementation of a master plan process.

B. FUNDING

The development of an economic strategy utilizing private and government grants for the capital improvements should be given a priority. The development of project phasing funding is usually a one to two year commitment for initial resources to become available. The primary funding source could be available through transportation funding opportunities. Recreational trail resources tend to be fewer, and lower in overall funding and more competitive, than transportation funds. Transportation funding is essentially provided for bicycle commuting purposes. All future planning and documentation should make an emphasis on bicycle commuting, rather than recreational uses.

Funding for master planning and preliminary environmental documentation is available less often through agency grants than funds for "bricks and mortar" projects. Funding for the preliminary work should be provided by the City and District and joint venture options.

Potential funding sources for the Arroyo Simi Greenway project include:

- Proposition 84;
- Development Impact Fees;
- Local Bond Issues;
- Sponsorships



Appendix A
Workshop Comments

Appendix B
Bike Lane Additions

Appendix C
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Comments from the Public Workshop
held on April 26, 2007



VISIONING STUDY
Arroyo Simi Greenway

Public Workshop Results

A public workshop was held on April 26, 2007 to describe the Arroyo Simi Greenway Visioning Study to the community of Simi Valley. The workshop included a presentation of the project and maps and images of the Conceptual Visioning Plan and some of the primary proposed enhancements. The workshop attendees were very enthusiastic about the plans and interested in the revitalization opportunities along the Arroyo Simi. The community and Simi Ad Hoc Committee comments are listed below. "General Workshop Notes" were ideas discussed informally during public review of the maps and sketches, and "Individual Public Comments" and "Simi Ad Hoc Committee Final Comments" were addressed to all attending.

General Workshop Notes:

- Fossilized riverbed exposed at Tapo Street – good interpretive exhibit location;
- Hummingbird Trail = Indian areas;
- ADA access important;
- Frontier Park currently has good ADA access;
- Mitigate neighborhood nuisances – landscape screening;
- Contact Rails-to-Trails to see if they will help support;
- Remove/ install fences - concerns with safety;
- Railroad at Oak Park has an arched tunnel for drainage - possible connection;
- Wildlife corridors must be maintained;
- 100 year flood must be protected;
- EPA herbicides - conflict with native vegetation;
- Flooding - maintain bridge heights – don't catch vegetation;
- Preserve the history of the area



Individual Public Comments:

- Supports plan -uses bike path, likes project;
- Clean up litter first – steep/deep/water year round;
- Shopping cart cleanup;
- Homelessness and garbage. Under bridge by train station at Los Angeles Avenue;
- Need police surveillance and patrolling;
- Sliding gates not locked and ladder going down Tapo Street at Gardia;
- Parkland next to houses - do not attract people. Decorate with trees, but not benches! Don't add things to sit on;
- Lighting – worried if too bright - too bright on Guardian Street;
- Nighttime noise/ drinking – worried about gathering near homes;
- Likes the Arroyo not being too busy;
- Consider separating pedestrian and bike traffic;
- Two sides to the Arroyo = different uses;
- Bike rider group – Tuesday morning rides;
- Fix path first;
- Weeds, mounds, glass;
- Adopt a trail/bike path (sections);
- Near-term clean up first;
- Water features – flow all the time? Natural flow only;
- Likes what he sees – short term – fix existing path;
- Prop 84 qualified – available to assist;
- Long-term funding process;
- Representative of Assemblyman Smythe – keep us updated;
- Impressed, plans look good;
- Trail – can become the focal point of the community;
- Member of R.S. Trail Blazers;
- Asphalt not holding up well – maintenance/ground squirrels;
- Use concrete instead. Concrete will reduce maintenance;
- Skateboards main transportation;
- Strollers hard on asphalt;
- Avid user – 3 times per week – rich environment, but in sad shape;
- Vision is awesome – worth striving for;
- Wildlife habitat important;
- Sad environment (neglected right now);
- Access to schools great;
- County is spraying plants and grading, let natural growth improve water quality;
- County owned areas using horse manure;
- Member of Heal the Bay, article showed Arroyo Simi as “bad” example;
- Royal Avenue: Green scum that goes to the ocean;
- We need a better quality of water;
- Bike path/access ideas good;
- Arroyo needs workout equipment – like Marina del Rey;
- Access/teens – needs patrols on bikes, lunch/after school/evenings;
- More people = better;
- Please more emphasis on environmental education;
- Educator – Environmental education focus needs to be more in forefront;
- Aesthetic/fitness value and education;
- Teach future generations



The community was able to review the Conceptual Vision Plan and sketches of proposed enhancements

Simi Ad Hoc Committee Final Comments:

Tim Shannon

- Very pleased, on right track;
- Clean up is part of process – vision is the answer;
- Natural learning about habitat
- Make a place for families;
- Police bike patrol – yes;
- Grading – flood control channel east of Sequoia is a training area– will remain

Elaine Freeman

- This evening brought energy to process;
- Likes idea of adopt-a-trail program;
- Make trail safer and more usable;
- Need mile markers to be implemented;
- Outreach to business - Bike to work. health and fitness, lunch power walks

Mark Johnson

- A lot of great ideas;
- Please keep ideas coming

Michael McGuigan

- Maintenance -fix path/adopt path;
- Concrete versus blacktop;
- Cleanup/homeless – more activity with families using remote areas, and homeless problem will be improved



Steve Sojka

- Nice to hear from public;
- Bringing people and families to Arroyo – take back the Arroyo;
- Safe route to schools/ parks;
- Potential for exercise equipment;
- Next – we will prioritize projects and talk to agency;
- Will take time;
- Start where there is the most impact;
- Grants for alternative transportation



The community was able to ask questions and express concerns to the Simi Ad Hoc Committee and RRM Design Group





Appendix A
Workshop Comments

Appendix B
Bike Lane Additions

Appendix C
Vision Plan Details

Appendix Description

Recommended Class II or III
Bike Lane Additions





VISIONING STUDY
Arroyo Simi Greenway



Class II and III bike lanes are very important to the future success of the Greenway, and provide vital connectivity throughout the City of Simi Valley to connect and provide access from surrounding schools, parks, neighborhoods. Class II bikeway lanes will require enough roadway width to add a separated striped lane for one-way bike travel on the street. In some cases, the City may pursue changing a road to a one-way automobile traffic, or remove existing on street parking to allow for bike lanes. Where space does not permit separated bike lanes, a Class III bike lane is recommended which does not have striped lanes, but does have highly visible signage depicting the roadway as a bike route. Class III bike lanes are only advisable on roadways with low volumes of traffic.



Class II and III Bike Lanes can significantly increase connections from the Greenway to the surrounding community





The following roadways are suggested in the Conceptual Vision Plan as potential for Class II or III bike lane additions, but require additional study to determine feasibility. Roadways identified in the Bicycle Master Plan suggested Class II bike lanes are identified with a (II), and recommended Class III bike lanes are identified with a (III).

- Alviso Street;
- Appleton Road;
- Aristotle Street (III);
- Aurelia Street;
- Bridget Avenue;
- Church Street;
- Cochran Street (III);
- Crosby Avenue;
- Dusan Street;
- East Los Angeles Avenue (II);
- Electra Avenue;
- Emory Avenue;
- Erringer Road (III);
- First Street (II);
- Fitzgerald Road (II);
- Fourth Street;
- Gibson Avenue;
- Harrington Road;
- Heywood Street;
- Hidden Ranch Drive;
- Katherine Road;
- Kuehner Drive (II);
- Madera Road (II);
- Menlo Street;
- Moreland Road;
- Planetree Avenue;
- Racine Street;
- Rivera Street;
- Royal Avenue (III);
- Santa Susana Pass Road (II);
- School Street;
- Sinaloa Road (III);
- Smith Road (III);
- Socrates Avenue;
- Stearns Street (II/III);
- Strathearn Place;
- Sycamore Drive (III);
- Ulysses Street;
- Union Place;
- Waldo Avenue;
- Yosemite Avenue (III)



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Workshop Comments

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Bike Lane Additions

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Complete explanation of the
Conceptual Vision Plan
Maps of the Conceptual Vision Plan

CONCEPTUAL VISION PLAN DETAILS

Appendix C



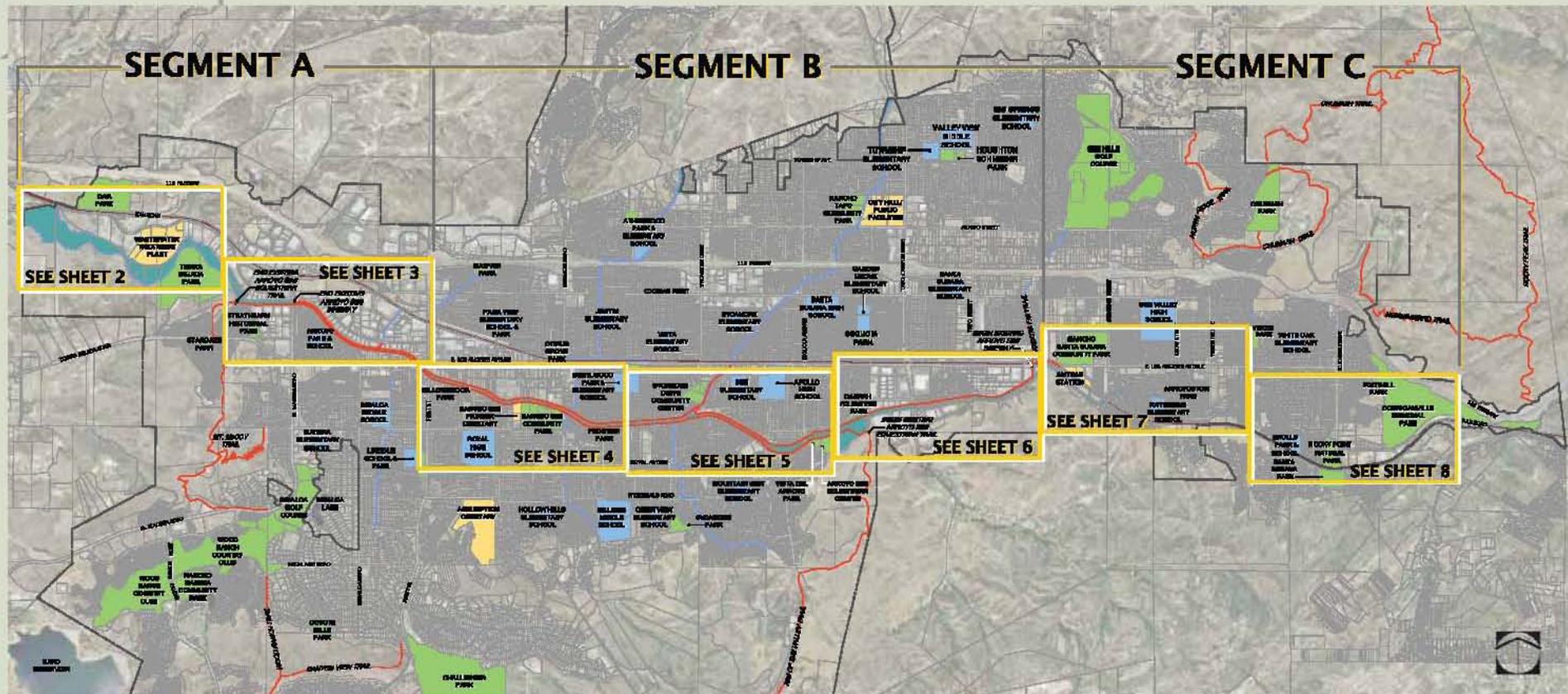


VISIONING STUDY
Arroyo Simi Greenway

The Conceptual Vision Plan has been broken into three segments to carefully consider the opportunities and constraints of each segment, and develop suggested improvements along the entire Arroyo Simi Greenway.

Suggested improvements below refer to the Conceptual Vision Plan, and are based on the Guiding Principles. Some principles are general and refer to overall ideas that should be included in the plan such as providing consistent fencing and landscaping, developing programs to control graffiti and trash, maintaining Flood Control functions, providing guidelines for adjacent development, and encouraging adjacent property owners to participate. Some elements of the Guiding Principles are difficult to locate on the plan at such a schematic level such as locating lighting at key points, and incorporating kiosks, interpretive signage and public art.





Conceptual Vision Plan
Overall Plan
Sheet 1

LEGEND

	ARROYO SIMI		CITY LIMITS
	PUBLIC PARKS		RAILROAD
	PUBLIC SCHOOLS		EXISTING TRAILS
	PUBLIC FACILITIES		ARROYO SIMI OPEN DRAINAGES



A photograph of the vacant parcel between Madera Road and East Los Angeles which offers a potential park location

A. SEGMENT A - CITY LIMITS LINE TO EAST LOS ANGELES AVENUE

Opportunities

This segment has the opportunity to connect the Arroyo Simi Greenway to a future regional trail in the City of Moorpark, and become part of the potential "Ultimate Loop" connecting with Ventura and Los Angeles Counties. This segment is bordered by multiple businesses, parks and schools and offers a wonderful opportunity to provide safer routes of travel for Simi Valley residents.

Opportunities in Segment A include connections to Oak Park and Tierra Rejada Park, both of which are underused and offer enhancement potential. Tierra Rejada Park is located close to the Arroyo Simi and could become a wonderful future destination. Another potential destination includes Strathearn Historical Park and Museum. The existing parking lot at Strathearn is large enough to allow for trail use parking, and the park has existing restroom facilities. A vacant parcel along the Arroyo Simi between Madera Road and East Los Angeles that provides an opportunity to develop an additional pocket park, and the potential to include ADA parking areas for the trail system.



Additional connection opportunities include a 15' wide trail easement on City property at the Wastewater Treatment Plant that provides a potential connection to West Los Angeles Avenue. The south side of the Arroyo provides an easy connection to the regional Mt. McCoy Trail. Street crossings in the segment are safe and include a signalized crossing at Madera Road and an under crossing at East Los Angeles. There is an opportunity to connect the south side of the city by Class I bikeway at Sinaloa Road and along the existing drainage on 5th Street which leads to Sinaloa Middle School.



Poor industrial views in this segment could benefit from landscape screening and enhancement

Constraints

Constraints in this segment include the odor and dust and lack of a bridge at the drainage crossing at the Wastewater Treatment Plant, and dense vegetation and lack of existing trail west of Madera Road. There are poor industrial views in this segment, as well as large blank walls on the under crossing between First Street and East Los Angeles Avenue.

Suggested Improvements for Segment A

1. Provide for Better Access and Connections

• Realign and extend existing path

- A Class I trail is suggested on the north side of the Arroyo Simi from the city limits line to west side of Madera Road. This trail does not currently exist and will require coordination with Flood Control, and may require environmental permits.
- The Class I trail on the north side of the Arroyo Simi east of Madera Road will remain.
- The dirt equestrian trail on the south side of the Arroyo Simi can remain unpaved from the west side of Madera Road, and should be extended up to Tierra Rejada Park to connect with the regional Mt McCoy Trail. If Tierra Rejada Park is improved, the dirt path should become paved.
- Additional connection improvements in this segment include Class II or III bike lanes listed in Appendix B of this document.

• Connect Greenway to parks, schools, neighborhoods, shops and restaurants

- The plan incorporates the 15' wide trail easement on City property at the Wastewater Treatment Plant as a connection to West Los Angeles Avenue.
 - A connection off West Los Angeles Avenue at the Oak Park entrance would further enhance park linkages.
 - A Class I trail could connect to the south side of the Arroyo at Sinaloa Road, and connect into a Class II or III bike lane through a easement on private property.
 - A Class I trail could extend along the existing Bus Canyon Drain at 5th Street and connect the Greenway to Sinaloa Middle School.
 - A Class I trail along the North Simi Drain could connect to the north side of the Arroyo from East Easy Street.
- ##### **• Develop trailhead areas**
- A trailhead is suggested at the existing parking lot at Strathearn Historical Park. It is a wonderful destination for trail users to learn about the region's rich historical past, and has existing public restrooms. Directional signage with maps is recommended to guide trail users to the Greenway.





- **Provide pedestrian/ bike bridges at key locations**

- A bridge is necessary at the drainage crossing at the Wastewater Treatment Plant.
- A bridge crossing could connect to the existing drainage along 5th Street which leads to Sinaloa Middle School.
- The two existing bridges located east of the Waste Water Treatment Plant on the south side of the Arroyo Simi should be enhanced to match the proposed style of the new bridges.

- **Provide additional rest areas and overlooks**

- Oak Park and Tierra Rejada Park should become destinations.
- Tierra Rejada Park could incorporate additional equestrian facilities and/ or passive uses such as an arboretum or native garden. Potential future uses of the park must fit the tone and theme of the Greenway, and construction and maintenance issues resolved. If the park is improved, good access will be needed.
- A privately owned vacant parcel along the Arroyo Simi between Madera Road and East Los Angeles and south of Easy Street, contains an existing trail connection and provides an opportunity to develop an additional pocket park. There is the potential to include ADA parking areas on this parcel for the greenway trail system. This parcel requires acquisition by the City.

2. Improve the Aesthetics

- **Protect and enhance the creek habitat**

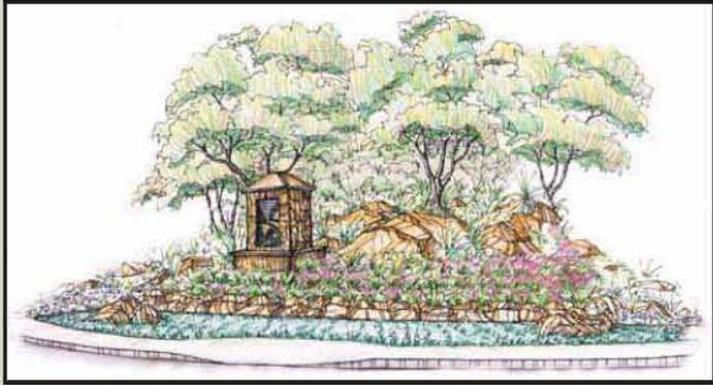
- Many areas along the greenway could benefit from restoration; however, specific areas seen as being highly visible from the roadway and have been shown as high priority habitat enhancement areas:
 - East and west of Madera Road;
 - From the west side of East Los Angeles Avenue to First Street
- The habitat west of Madera Road and south of the Waste Water Treatment Plant is lush and very dense in areas, and should be carefully protected and maintained.

- **Provide consistent fencing and landscaping**

- Design Guidelines should be developed along the Arroyo Simi to develop a sense of place for the greenway to create consistent and uniform palette for adjacent residential and commercial uses.

- **Shield bad views and highlight desirable areas**

- Landscape screening and enhancement is recommended throughout this segment to minimize the poor industrial views; specifically the area on the north side of the Arroyo west of First Street.
- The odor and dust should be minimized through landscape screening, fragrant plants, and fencing along the Wastewater Treatment Plant.

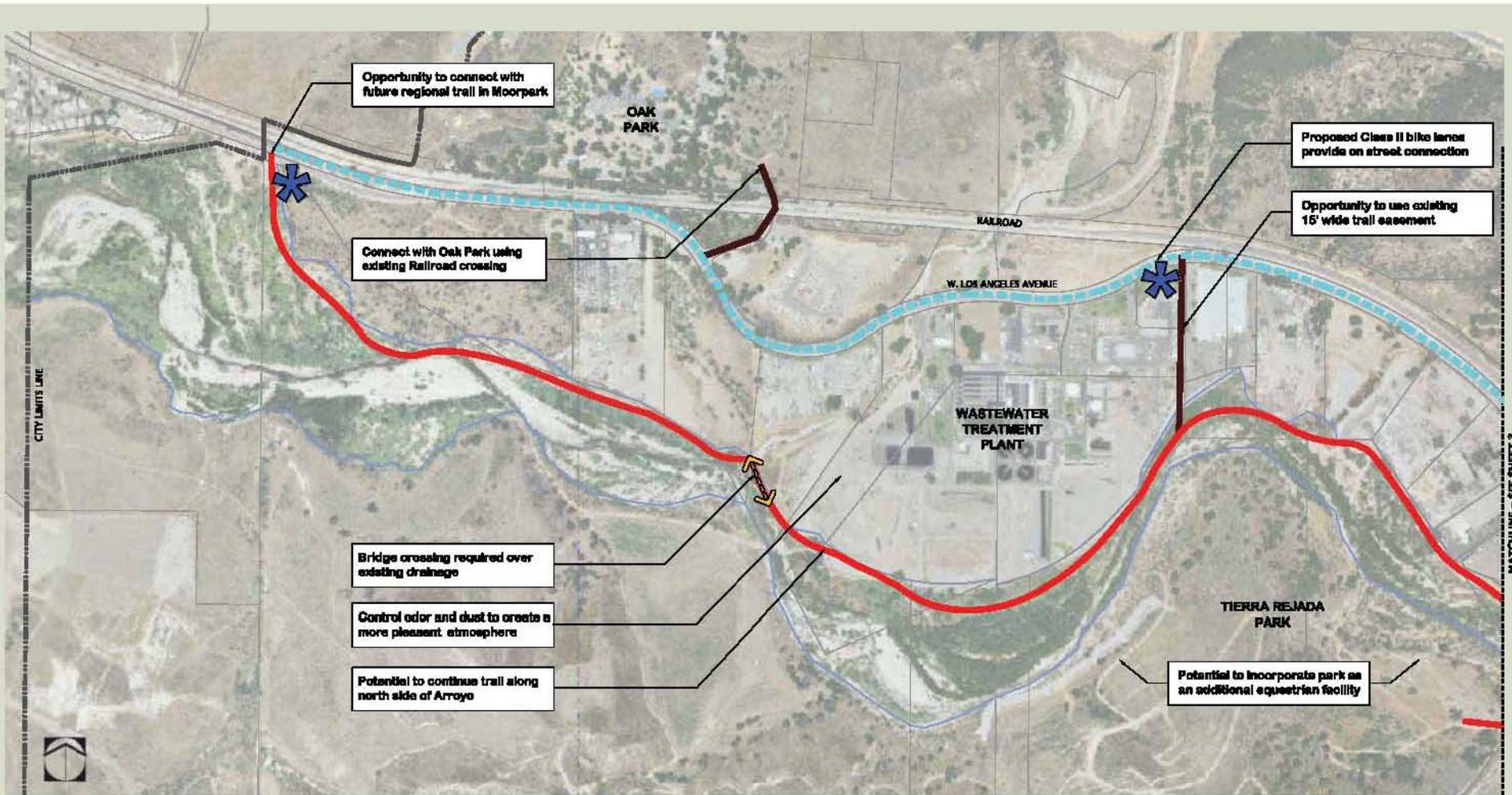


Enhanced entries with gateway signage will improve the appearance of the Arroyo Simi Greenway

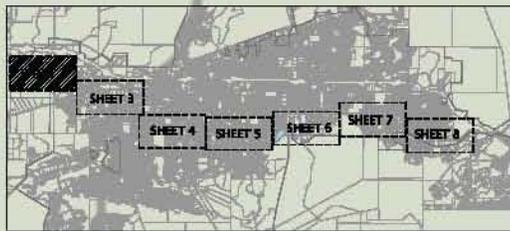
- **Improve existing bridge facades and streetscapes**
 - Existing under crossing and large blank walls at East Los Angeles Avenue should incorporate bridge façade enhancements described in Chapter 5 of this Visioning Study.
- **Create gateways and enhance entries**
 - All entries to the Greenway should be enhanced as discussed in Chapter 5 of this Visioning Study.
 - Madera Road is a major entry point and highly visible from passing traffic, and this intersection should be significantly enhanced and highlighted as a primary gateway.

3. Improve Safety Perception and Stewardship

- **Create better visibility and signage**
 - Existing and proposed entries at the city limits line, Madera Road, and East Los Angeles Avenue should incorporate trail entry and signage enhancement as described in Chapter 5 of this Visioning Study.
 - There is opportunity for a secondary trail entry at the 15' wide easement adjacent to Wastewater Treatment Plant, and although it is not directly located along the greenway, it can add to the overall sense of entry and ease of connections.
- **Construct street under crossings**
 - The clearance under the Madera Road Bridge offers the potential for a future under crossing.



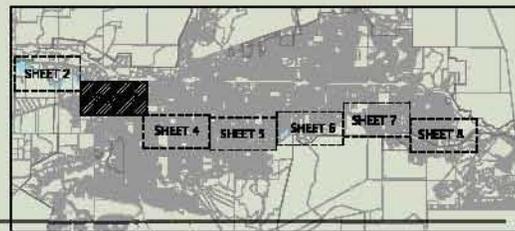
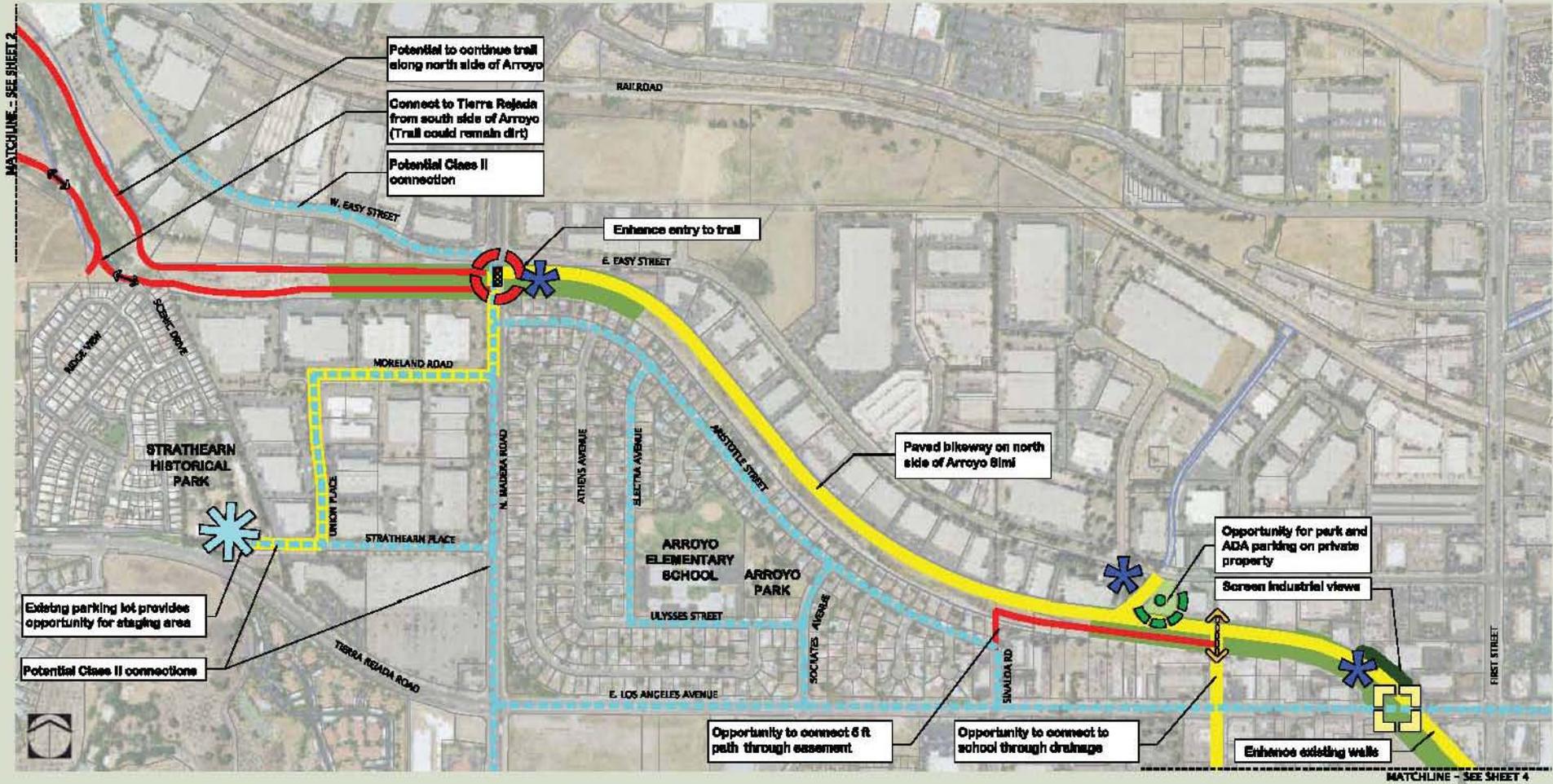
MATCHLINE - SEE SHEET 3



LEGEND

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|--|-------------------------------|--|------------------------------------|
| | Existing At Grade Crossing | | Proposed Screening/ Enhancement |
| | Existing Bridge Undercrossing | | High Priority Habitat Enhancement |
| | Proposed Pocket Park | | Primary Trail Alignment |
| | Proposed Overlook/ Rest Area | | Secondary Trail Alignment |
| | Proposed Bridge | | Connector Trail Alignment |
| | Existing Bridge | | Existing Regional Trail |
| | Proposed Trailhead/ Parking | | Ultimate Class II or III Bike Lane |
| | Proposed Gateway | | Proposed Interpretive Exhibit |

Conceptual Vision Plan
Segment A
Sheet 2



LEGEND

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|--|-------------------------------|--|------------------------------------|
| | Existing At Grade Crossing | | Proposed Screening/Enhancement |
| | Existing Bridge Undercrossing | | High Priority Habitat Enhancement |
| | Proposed Pocket Park | | Primary Trail Alignment |
| | Proposed Overlook Rest Area | | Secondary Trail Alignment |
| | Proposed Bridge | | Connector Trail Alignment |
| | Existing Bridge | | Existing Regional Trail |
| | Proposed Trailhead/Parking | | Ultimate Class II or III Bike Lane |
| | Proposed Outway | | Proposed Interpretive Exhibit |

Conceptual Vision Plan
Segment A
Sheet 3



B. SEGMENT B - EAST LOS ANGELES AVENUE TO AMTRAK STATION

Opportunities

This segment is rich in opportunities and includes many potential trail and park connections. This segment connects to the Rim of the Valley Trail east of the Arroyo Simi Equestrian Center and the Kadota Fig Trail north of Angus Avenue, and offers additional regional trail connections. This segment is primarily bordered by residents, parks and schools and offers a wonderful opportunity to provide safer and more scenic routes of travel for Simi Valley residents.

Opportunities include the multiple parks and the Sycamore Drive Community Center adjacent to the Arroyo Simi. Willowbrook Park, Rancho Simi Community Park, Frontier Park, Vista del Arroyo Park, Arroyo Simi Equestrian Center, and Darrah Volunteer Park all exist directly adjacent to the Arroyo Simi. The existing parking lot at Rancho Simi Community Park and the Sycamore Drive Community Center are large enough to allow for trail use parking, and have existing restroom facilities.

Several vacant parcels along the Arroyo Simi provide opportunities to develop additional pocket parks and overlook/rest areas. There is ample vacant land around Darrah Volunteer Park and Arroyo Simi Equestrian Center, and there is potential for significant park expansion in these areas. There is opportunity for trail entry enhancement by Angus Avenue and north of the Sycamore Drive Community Center. There are two existing cascades between First Street and Erringer Road which offer areas for habitat enhancement, pedestrian and bicycle improvements, and bridge crossing potentials. Existing overlooks and rest areas at Royal Avenue and Tapo Canyon Road offer enhancement opportunities.



Large blank walls within this segment detract from the Greenway experience, and could be minimized by landscape screening

There is an opportunity to connect to the Arroyo Simi along the existing drainages east of Erringer Road and Sycamore Drive, and the potential to incorporate a trail in an existing planting area along East Los Angeles Avenue east of Sycamore Drive and extend the trail up the existing drainage up to Cochran Street. There is also an opportunity to incorporate a trail connection along the power line easement east of Darrah Volunteer Park and through Rancho Simi Community Park and Sycamore Drive Community Center. There is the potential for a bridge connection to link Simi Elementary and Apollo High Schools to the Arroyo Simi via School Street. Street crossings in the segment are primarily safe and include signalized crossings and an under crossings at First Street and Tapo Street. Existing bridges at Erringer Road, Sycamore Drive, Royal Avenue and Sequoia Avenue have enough vertical clearance to potentially provide future under crossings opportunities.

Constraints

Constraints in this segment include large blank walls at the existing under crossing and a mishmash of fence types, and poor views which are in need of landscape screening/ enhancement. Also many of the parks are located on the south side of the Arroyo Simi, and not easily accessible from the existing bikeway. Two unsignalized crossings at Sequoia Avenue and Tapo Canyon Road are safety constraints to trail users.



Suggested Improvements for Segment B

1. Provide for Better Access and Connections

- **Realign and extend existing path**

- A Class I trail is proposed on both the north and south side of this segment of the Arroyo Simi Greenway.
- The Primary Trail will be located along the south side of the Arroyo, and it does not currently exist. It will require coordination with Flood Control and may require environmental permits.
- Additional connection improvements in this segment include Class II or III bike lanes listed in Appendix B of this document.

- **Connect Greenway to parks, schools, neighborhoods, shops and restaurants**

- Class I trail is suggested along the existing Dry Canyon Drain east of Erringer Road which extends from north side of Arroyo Simi to Heywood Street, and has the potential to connect to Berylwood Park and Elementary School by a Class II or III bikeway.
- There are Class I trail connections suggested through Rancho Simi Community Park and Sycamore Drive Community Center to provide safe and efficient access to the greenway.
- Class I trail is recommended along the existing Runkle Canyon Drain east of Sycamore Drive to connect from south side of the greenway to Fitzgerald Road and link to four schools on Class II or III bikeways.

- A Class I trail is proposed in an existing planting area along East Los Angeles Avenue, east of Sycamore Drive, which can connect along the existing Tapo Canyon Drain and extend from the north side of the railroad up the drainage to a Class II or III bikeway on Cochran Street, and has the potential to connect to Sycamore Elementary School.
 - Class I trail is suggested along the existing east of Erringer Road north of East Los Angeles, extending from a Class II or III bikeway on Erringer along the drainage along the north side of the railroad extending up to Cochran Avenue, and connecting to Justin Elementary School.
 - A Class I trail is proposed along the power line easement east of Darrah Volunteer Park which connects to a Class II or III bikeway on East Los Angeles Avenue to provide additional access to the greenway.
 - A Class I trail is also suggested along the power line easement west of Tapo Canyon Road and south of Cochran Street which connects to a Class II or III bikeway on Cochran and connects to Garden Grove Elementary School and Sequoia Park.
- **Develop trailhead areas**
 - Trailheads are recommended at the existing parking lots at Rancho Simi Community Park and the Sycamore Drive Community Center because they are large enough to allow for trail use parking, and have existing restroom facilities.

- **Provide pedestrian/ bike bridges at key locations**

- The existing cascades between First Street and Erringer Road offer areas for pedestrian and bicycle improvements and bridge crossing potential.
- There is the opportunity for a drainage crossing east of the Sycamore Drive Community Center, south of East Los Angeles Avenue, connecting to a Class II or III bike lane on School Street to link Simi Elementary and Apollo High Schools to the Arroyo Simi.
- A bridge connection south of Sycamore Drive Community Center can create an easy transition across the greenway.
- A bridge at the proposed park expansion area south of Darrah Volunteer Park will connect the parks, greenway, and the regional trail system.
- The existing bridge south of the Sycamore Drive Community Center should be enhanced to match the proposed style of new bridges along the greenway.

- **Provide additional rest areas and overlooks**

- Connections along the Arroyo Simi are suggested to the multiple parks adjacent to the Arroyo Simi to further enhance recreational potential:
 - Willowbrook Park;
 - Rancho Simi Community Park;
 - Frontier Park;
 - Gateway Park;
 - Vista del Arroyo Park;
 - Arroyo Simi Equestrian Center;
 - Darrah Volunteer Park

- Several publicly owned vacant parcels along the Arroyo Simi provide opportunities to develop additional pocket parks:
 - North of Rancho Simi Community Park on south side of greenway (create a better connection to the existing park);
 - East of Sycamore Drive on north side of greenway;
 - There are two parcels of vacant land southwest of Darrah Volunteer Park and east of Arroyo Simi Equestrian Center, and there is potential for significant park and equestrian expansion in these areas;
 - The vacant property east Angus Avenue is a wonderful location for a pocket park, and its visibility from the road will increase the perception from the roadway as a greenway with park areas and provide a great destination and meeting area
- Several privately owned vacant parcels along the Arroyo Simi provide opportunities to develop additional pocket parks, yet require acquisition or easements:
 - West of Sycamore Drive on south side of greenway;
 - South of Royal Avenue on south side of greenway
- Existing overlooks and rest areas at Royal Avenue and Tapo Canyon Road offer enhancement opportunities.



- Areas along the Arroyo Simi offer potential locations for overlook/rest areas:
 - Existing cascades such as the one between Rancho Simi Community Park and Willowbrook Park on south side of greenway;
 - North of Royal Avenue on north side of greenway;
 - West of Sequoia Avenue on south side of greenway;
 - West of Tapo Canyon Road on north side of greenway;
 - South of Angus Avenue on north side of greenway

2. Improve the Aesthetics

• **Protect and enhance the creek habitat**

- Many areas along the greenway could benefit from restoration; however, specific areas seen as being highly visible from the roadway and have been shown as high priority habitat enhancement areas:
 - From First Street to East of Erringer Road;
 - East and west of Sycamore Drive;
 - West of Royal Avenue to Darrah Volunteer Park;
 - East and west of Tapo Canyon Road;
 - East and west of Tapo Street;
 - South of Angus Avenue ;
 - Two existing cascades between First Street and Erringer Road

• **Provide consistent fencing and landscaping**

- Design Guidelines should be developed along the Arroyo Simi to develop a sense of place for the greenway to create consistent and uniform palette for adjacent residential and commercial uses.

• **Shield bad views and highlight desirable areas**

- Poor views in this segment are in need of landscape screening/ enhancement. Primary areas for landscape screening/ enhancement are recommended in the following locations:
 - North of Rancho Simi Community Park;
 - West and east of Sycamore Drive;
 - North of Sycamore Drive Community Center;
 - East of Sequoia Avenue;
 - South of Darrah Volunteer Park;
 - East of Angus Avenue

• **Incorporate kiosks, interpretive signage and public art**

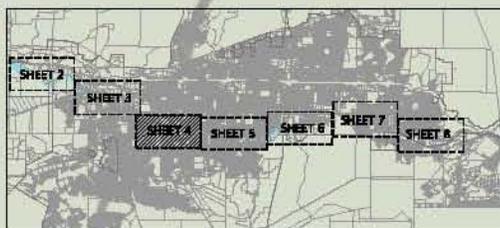
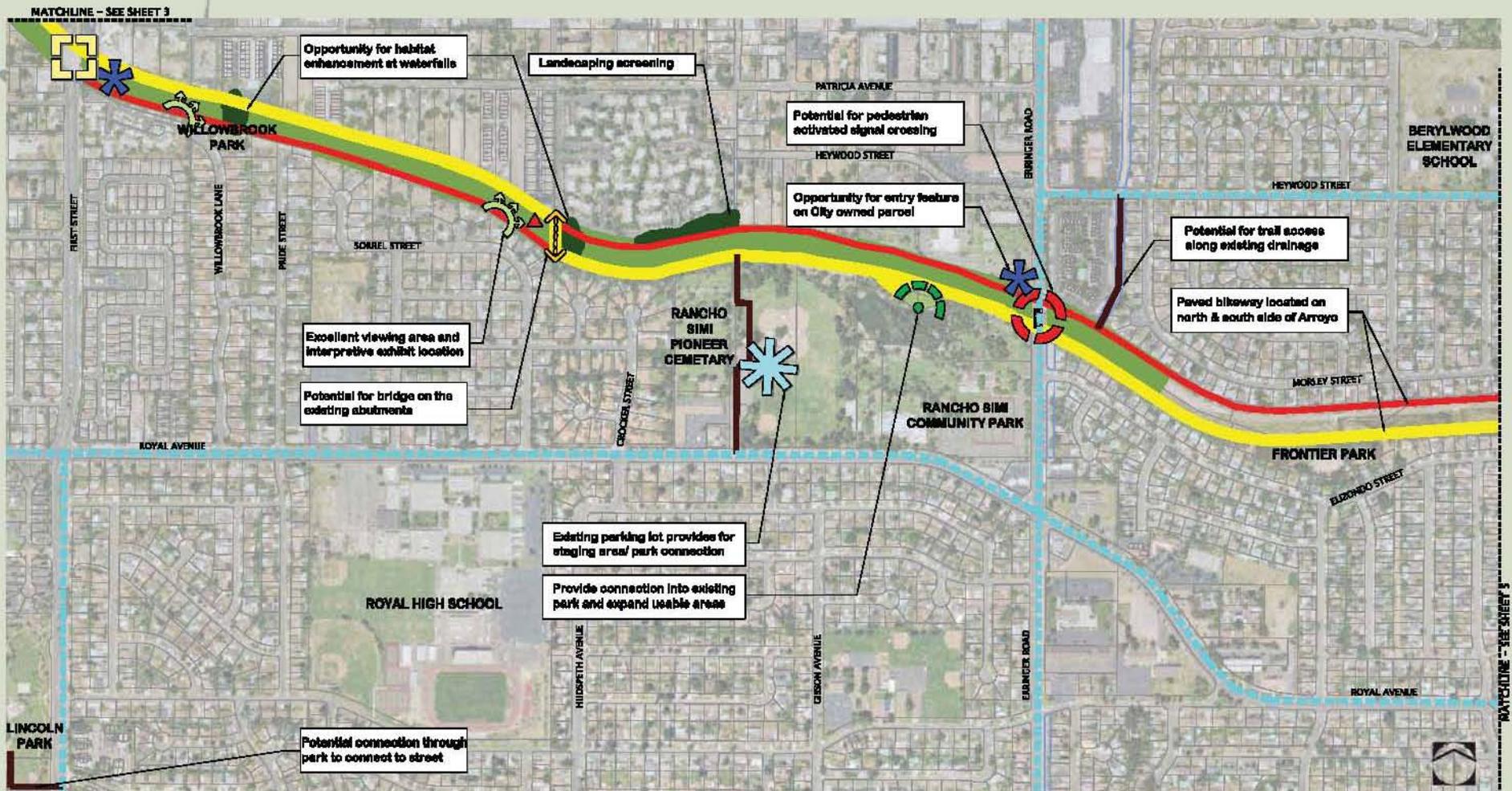
- Interpretive exhibits are recommended to be placed at strategic locations along the Arroyo Simi, and additional areas along the greenway could benefit from interpretive exhibits; however, specific areas seen as needing them are:
 - The existing cascade between First Street and Erringer Road offers areas for environmental interpretive exhibits;
 - The park expansion area southwest of Darrah Volunteer Park has potential for interpretive exhibits;

- Vista del Arroyo Park could incorporate educational exhibits geared towards children;
- The proposed overlook/ rest area south of Angus Avenue has could have a historical interpretive exhibit about the former airport
- **Improve existing bridge facades and streetscapes**
 - Existing under crossings at First Street and Tapo Street should incorporate bridge façade enhancements described in Chapter 5 of this Visioning Study.
- **Create gateways and enhance entries**
 - All entries to the Greenway should be enhanced as discussed in Chapter 5 of this Visioning Study.
 - Erringer Road is a major entry point which is highly visible from passing traffic, and should be significantly enhanced and highlighted as a primary gateway. The city owns a parcel adjacent to the north side of greenway, west of Erringer, and a major entry feature is suggested in this area.
 - The entry east of Angus Avenue is a key entry point which is visible from passing traffic, and should be significantly enhanced and highlighted as a primary gateway.

3. Improve Safety Perception and Stewardship

- **Create better visibility and signage**
 - Existing and proposed entries at First Street, Erringer Road, Sycamore Drive, Royal Avenue, Sequoia Avenue, Tapo Canyon Road, and Tapo Strret should incorporate signage enhancement as described in Chapter 5 of this Visioning Study.
 - There is opportunity for secondary trail entries north of the Sycamore Drive Community Center and west of Darrah Volunteer Park, and they can add to the overall sense of entry.
- **Address street crossing issues**
 - Two unsignalized crossings at Sequoia Avenue and Tapo Canyon Road are safety constraints to trail users, and additional studies are recommended. Unsignalized crossings should incorporate the street crossing enhancements described in Chapter 5 of this Visioning Study.
 - Erringer Road is a heavily traveled roadway and although there is an existing traffic signal, a secondary pedestrian activated signal is suggested.
- **Construct street under crossings**
 - Existing bridges at Erringer Road, Sycamore Drive, Royal Avenue and Sequoia Avenue have enough vertical clearance to potentially provide future under crossings opportunities, but require additional study to determine the feasibility of future under crossings.

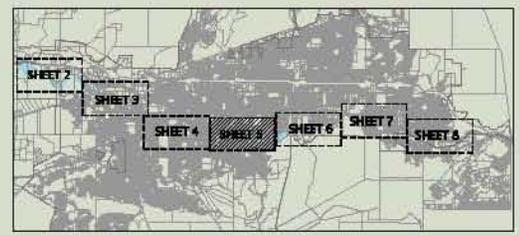
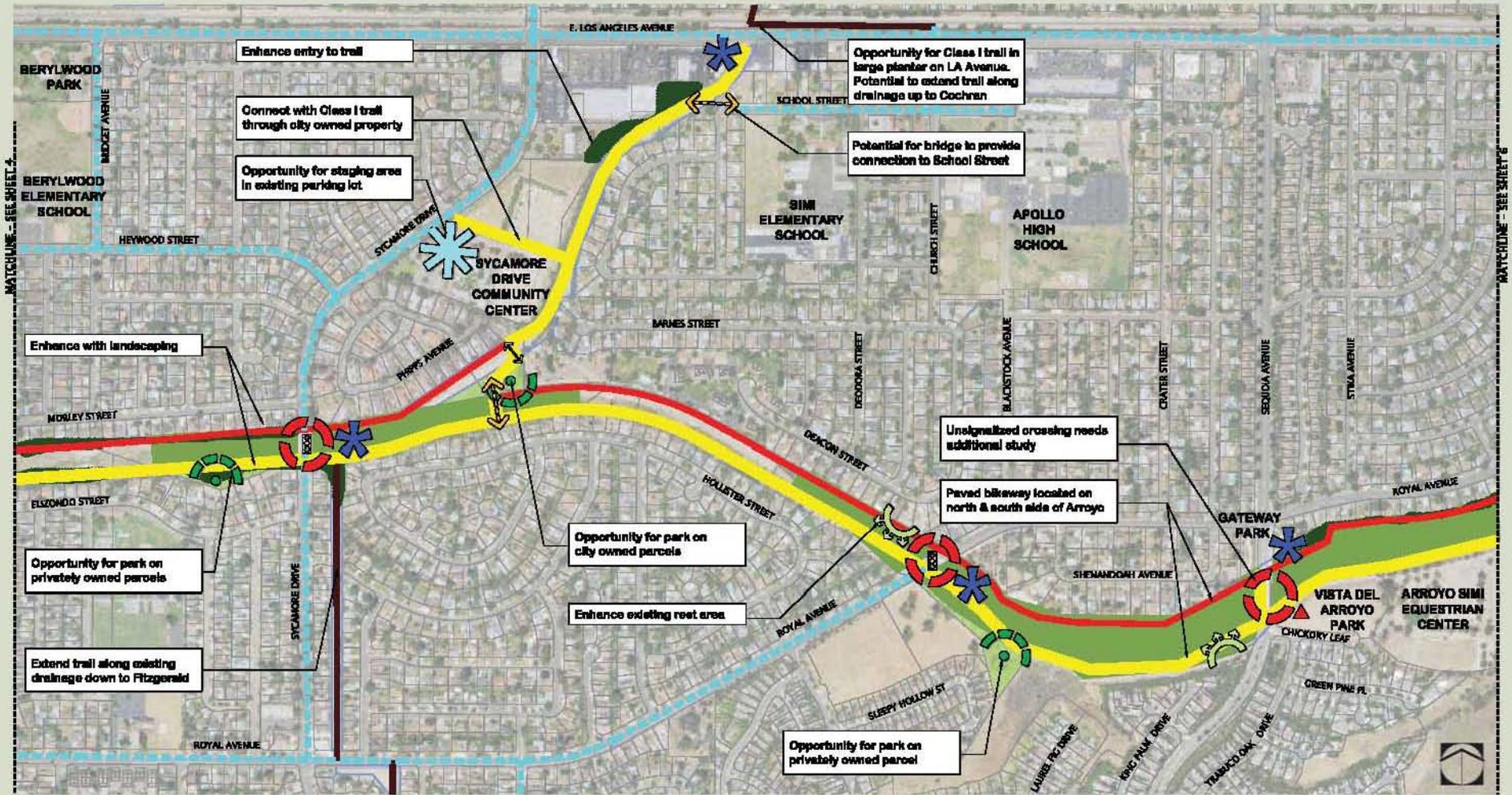




LEGEND

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|--|-------------------------------|--|------------------------------------|
| | Existing At Grade Crossing | | Proposed Screening/ Enhancement |
| | Existing Bridge Undercrossing | | High Priority Habitat Enhancement |
| | Proposed Pocket Park | | Primary Trail Alignment |
| | Proposed Overlook/ Rest Area | | Secondary Trail Alignment |
| | Proposed Bridge | | Connector Trail Alignment |
| | Existing Bridge | | Existing Regional Trail |
| | Proposed Trailhead/ Parking | | UR/roads Class II or III Bike Lane |
| | Proposed Gateway | | Proposed Interpretive Exhibit |

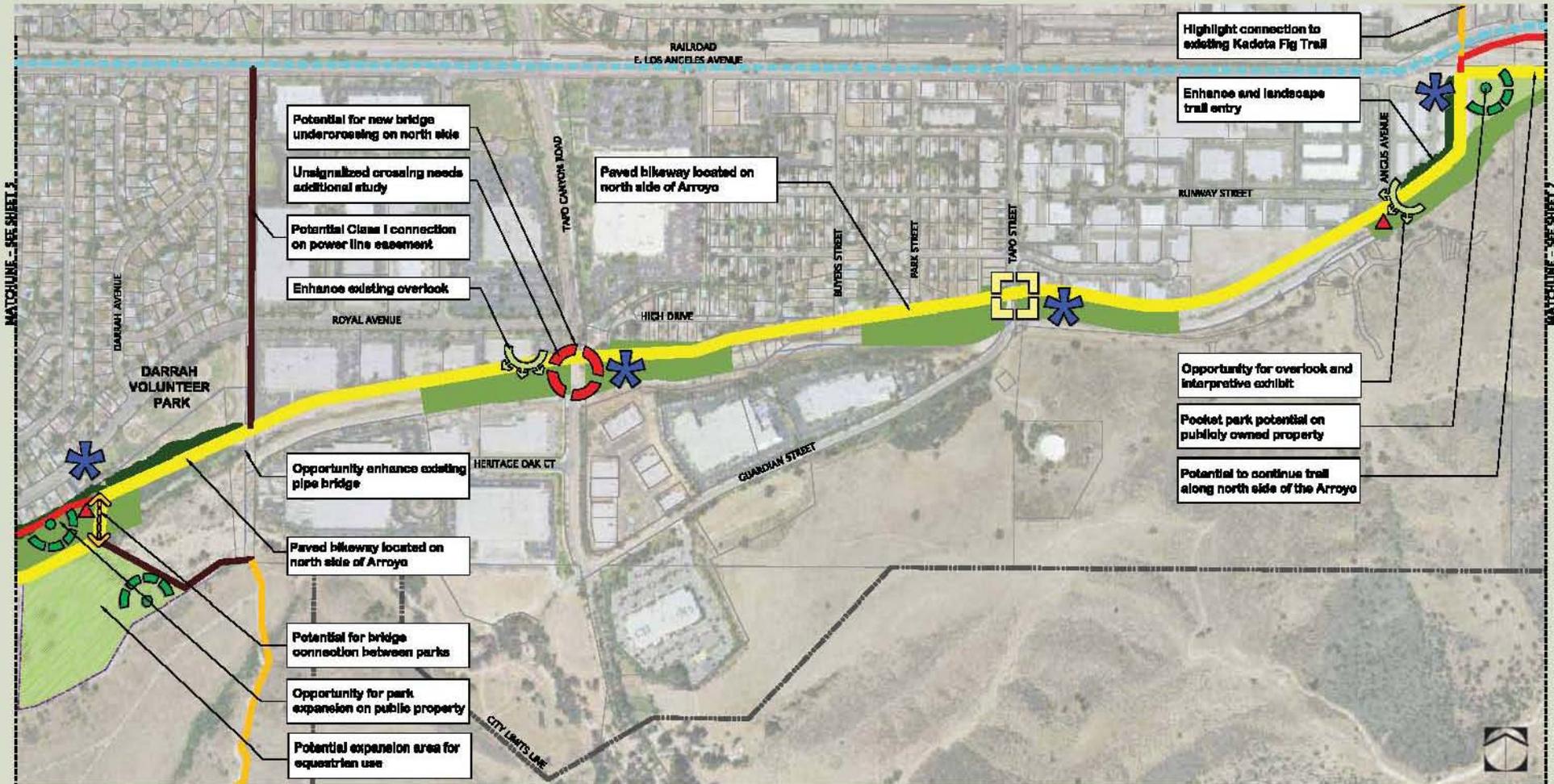
Conceptual Vision Plan
Segment B
Sheet 4



LEGEND

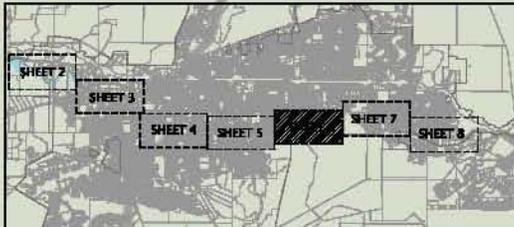
	Existing At Grade Crossing		Proposed Screening/Enhancement
	Existing Bridge Undercrossing		High Priority Habitat Enhancement
	Proposed Pocket Park		Primary Trail Alignment
	Proposed Overlook/Rest Area		Secondary Trail Alignment
	Proposed Bridge		Connector Trail Alignment
	Existing Bridge		Existing Regional Trail
	Proposed Trailhead Parking		Ultimate Class II or III Bike Lane
	Proposed Gateway		Proposed Interpretive Exhibit

Conceptual Vision Plan
Segment B
Sheet 5



LEGEND

- | | | | |
|--|-------------------------------|--|------------------------------------|
| | Existing At Grade Crossing | | Proposed Screening/ Enhancement |
| | Existing Bridge Undercrossing | | High Priority Habitat Enhancement |
| | Proposed Pocket Park | | Primary Trail Alignment |
| | Proposed Overlook/ Rest Area | | Secondary Trail Alignment |
| | Proposed Bridge | | Connector Trail Alignment |
| | Existing Bridge | | Existing Regional Trail |
| | Proposed Trailhead/ Parking | | Ultimate Class II or III Bike Lane |
| | Proposed Gateway | | Proposed Interpretive Exhibit |



Conceptual Vision Plan
Segment B
Sheet 6

C. SEGMENT C - AMTRAK STATION TO CORRIGANVILLE PARK

Opportunities

Corriganville offers a rich opportunity to connect the Arroyo Simi to its vast regional trail system to the north east. This segment is located adjacent to the Amtrak/ Metrolink Station, as well as residential areas and multiple schools and parks, and offers wonderful opportunities to provide safer routes of travel and commuting for Simi Valley residents.

This segment contains a very scenic portion of the Arroyo Simi which contains some dense vegetation and natural habitat area south of the Amtrak/ Metrolink Station extending up to the railroad east of Hidden Ranch Drive.

Opportunities in this segment include the ample connectivity potential. Arroyostow Park, Rocky Point Natural Park, and Corriganville Park are all located directly adjacent to the Arroyo Simi. Rancho Santa Susana Community Park, Knolls Park and Santa Susana Park are situated in close proximity to the Arroyo Simi, and offer magnificent destination potentials. An existing tunnel under the railroad tracks between Santa Susana Park and Rocky Point Natural Park provide a potential railroad crossing opportunity. The Amtrak/ Metrolink Station is located close to the Arroyo Simi and is connected via a wide sidewalk along East Los Angeles Avenue, and offers a great commuting potential for trail users and residents.





Katherine and Knolls Elementary Schools are both located adjacent to the Arroyo Simi and have existing bridge connections over the Arroyo, and provide a wonderful safe route to school opportunity. The existing parking lots at the Amtrak/ Metrolink Station, Corriganville Park and the Santa Susana Park are large enough to allow for trail use parking, and Santa Susana Park has existing restroom facilities. Rancho Santa Susana Community Park and Knolls Park also have existing restroom facilities, and the opportunity to enhance their connections to the Arroyo Simi.

A bikeway in this area has been planned and engineered along the north side of the Arroyo from the end of the existing bikeway near Angus Avenue to Yosemite Avenue, and the entire drainage channel in this segment is wide enough to support a trail. There is an opportunity to connect to this segment of the Arroyo Simi to Kuehner Drive along the existing drainage east of Edgewood Drive. The railroad offers a potentially wide easement area to allow for trail use. There is the potential to enhance the trail connection along East Los Angeles between the Amtrak Station and Angus Avenue.



Photograph of the existing pedestrian bridge at Katherine Elementary School which is a great connection to the Arroyo Simi Greenway and safe route to school



The existing sidewalk on East Los Angeles narrows as it approaches the railroad crossing and creates trail use constraints

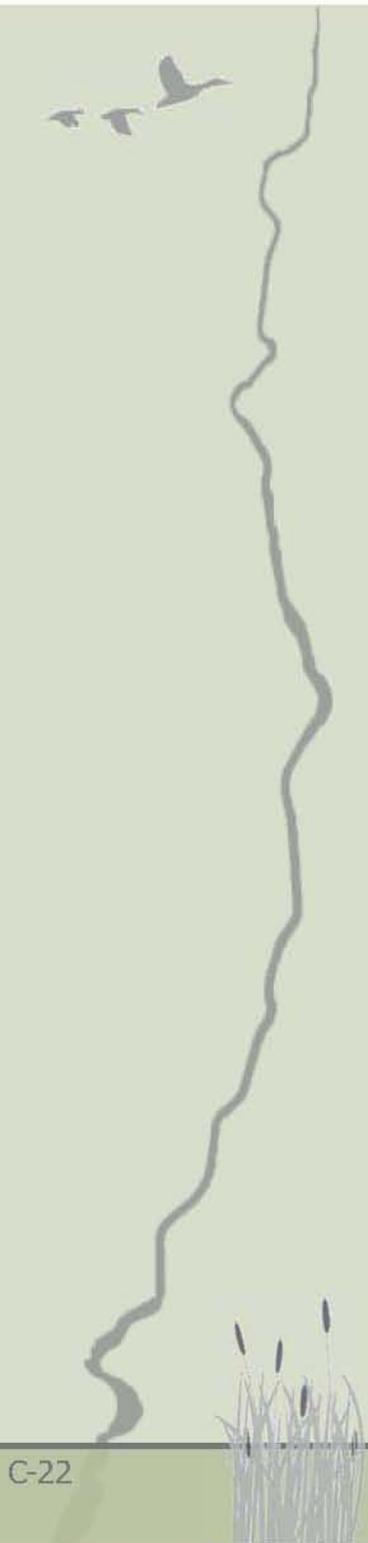
Constraints

Constraints in the segment include the fact that the trail has not yet been developed in this area. The Arroyo Simi is almost entirely concrete in this segment and has no vegetation within the channel area, except the area south of the Amtrak/ Metrolink Station described above. A railroad crossing constraints exists between Hidden Ranch Drive and Streamns Street, and a direct trail connection is very difficult due to existing bridge abutments. There are many unsignalized street crossings in this segment which provide safety constraints. The trail connection along East Los Angeles between the Amtrak/ Metrolink Station and Angus Avenue is a widened sidewalk which narrows as it approaches the railroad crossing, and creates safety and trail use constraints.

Suggested Improvements for Segment C

1. Provide for Better Access and Connections

- **Realign and extend existing path**
 - A Class I trail is suggested on the north side of the Arroyo Simi from the existing trail east of Angus Avenue to Yosemite Avenue, and on the south side of the Arroyo from Yosemite to Corriganville Park. A trail does not currently exist in this segment, and will require coordination with Flood Control and may require environmental permits.

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- A trail in this segment has been planned and engineered by the City of Simi Valley along the north side of the Arroyo from the end of the existing bikeway near Angus Avenue to Yosemite Avenue, and it has been incorporated into the Conceptual Vision Plan.
 - Additional connection improvements in this segment include Class II or III bike lanes listed in Appendix B of this document.

- **Connect Greenway to parks, schools, neighborhoods, shops and restaurants**

- The Class I sidewalk trail along East Los Angeles between the Amtrak/ Metrolink Station and Angus Avenue should be widened and enhanced with fencing and/or landscaping because it currently narrows and creates safety and trail use constraints.
- A potential Class I trail is proposed along the railroad to avoid on-street travel, but needs additional study and approval by the railroad.
- A Class I trail is proposed along the existing Hummingbird Creek and White Oak Creek drainages east of Edgewood Drive to connect to this segment of the Arroyo Simi to White Oak Elementary School and Kuehner Drive.
- Class I trails are proposed to connect the neighborhood street with Katherine and Knolls Elementary Schools, and provide a wonderful safe route to school opportunity.

- A Class I trail is proposed between Santa Susana Park and Rocky Point Natural Park, and incorporates an existing tunnel under the railroad to provide a connection between the parks.
- A Class I trail could run parallel along Kuehner Drive to connect to Smith Road to minimize on street travel.
- Corriganville offers rich opportunity to connect the Arroyo Simi Greenway to its vast regional trail system to the northeast.

- **Develop trailhead areas**

- Trailheads are proposed at the existing parking lots at the Amtrak/ Metrolink Station, Corriganville Park and the Santa Susana Park because they are large enough to allow for trail use parking, and Santa Susana Park has existing restroom facilities.

- **Provide pedestrian/ bike bridges at key locations**

- A bridge is proposed south of Edgewood Drive to connect the existing Hummingbird Creek drainage to the south side of the Arroyo Simi.
- The existing bridge adjacent to Katherine Elementary School should be enhanced to match the proposed style of the new bridges.

- **Provide additional rest areas and overlooks**

- Arroyostow Park, Rocky Point Natural Park, and Corriganville Park are all located directly adjacent to the Arroyo Simi and add to the connectivity and recreational potential.
- Rancho Santa Susana Community Park, Knolls Park and Santa Susana Park are situated in close proximity to the Arroyo Simi and offer magnificent destination potentials.

2. Improve the Aesthetics

- **Protect and enhance the creek habitat**

- The Arroyo Simi is primarily concrete in this segment and has no vegetation within the channel area. Coordination with Flood Control is necessary to determine the possibility of planting areas in this segment.
- There is a nice habitat area that could benefit from enhancement in the area south of the Amtrak/ Metrolink Station which extends up to the railroad east of Hidden Ranch Drive. It is lush and very dense in areas, and should be carefully protected and maintained.

- **Provide consistent fencing and landscaping**

- Design Guidelines should be developed along the Arroyo Simi to develop a sense of place for the greenway to create consistent and uniform palette for adjacent land uses.

- **Shield bad views and highlight desirable areas**

- Poor views in this segment should incorporate landscape screening/ enhancement.

- **Incorporate kiosks, interpretive signage and public art**

- Interpretive exhibits are recommended to be placed at strategic locations along the Arroyo Simi, and additional areas along the greenway could benefit from interpretive exhibits; however, specific areas seen as needing them are:
 - Rocky Point Natural Park offers areas for environmental interpretive exhibits;
 - Corriganville Park could have interpretive exhibits highlighting its rich and colorful movie-making history;
 - Santa Susana Park and Train Depot has potential for interpretive exhibits about train history

- **Improve existing bridge facades and streetscapes**

- Katherine and Knolls Elementary Schools have existing bridge connections over the Arroyo, and could be enhanced with bridge and wall treatments and/ or landscaping.

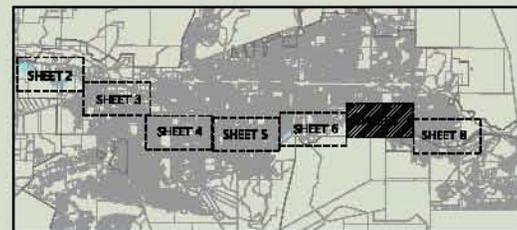
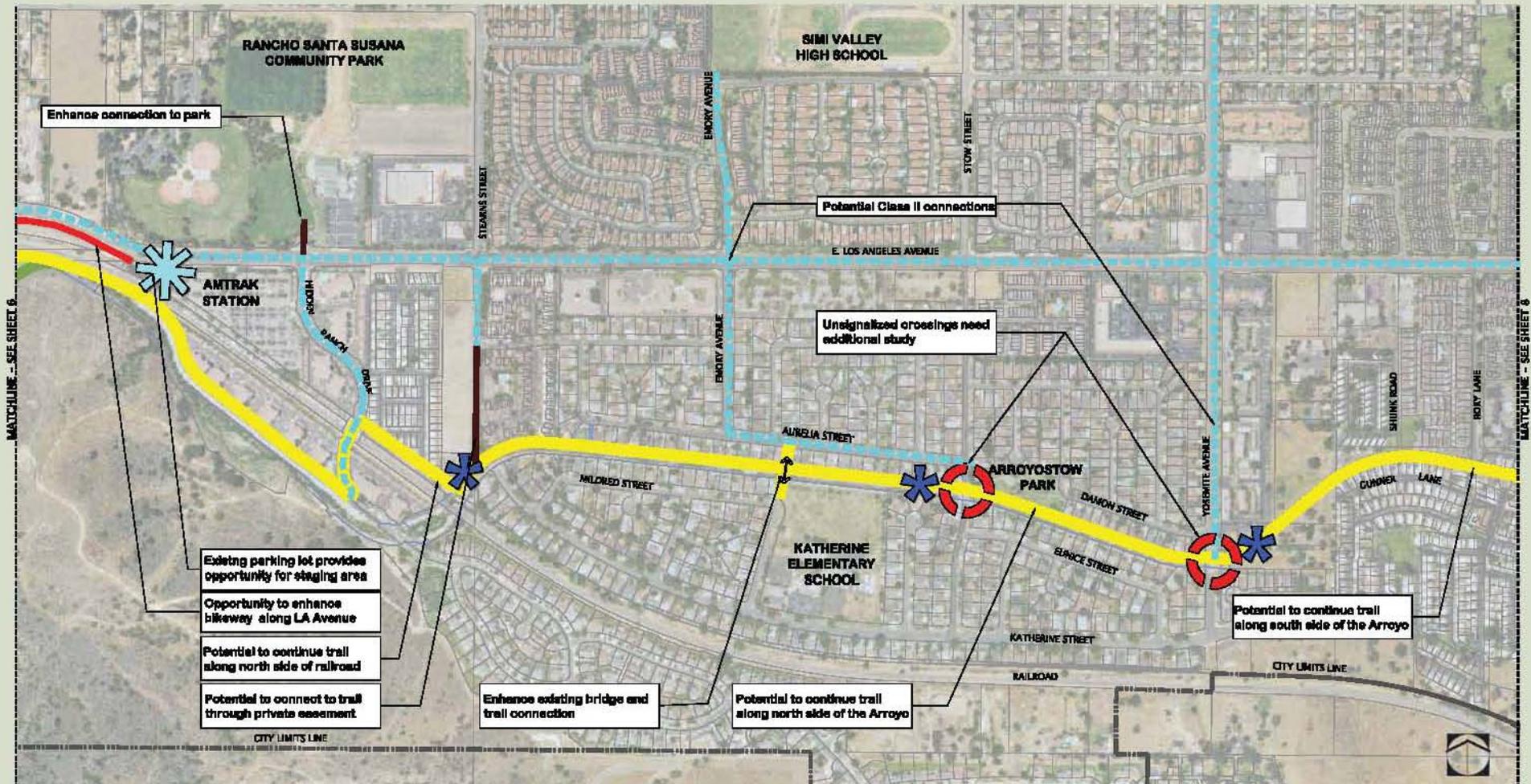


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- **Create gateways and enhance entries**
 - All entries to the Greenway should be enhanced as discussed in Chapter 5 of this Visioning Study.
 - Rocky Point Natural Park is a major entry point which is highly visible from passing traffic, and should be significantly enhanced and highlighted as a primary gateway.

3. Improve Safety Perception and Stewardship

- **Create better visibility and signage**
 - Proposed entries at Stow Street, Yosemite Avenue, Katherine Road, and Kuehner Drive should incorporate signage enhancement as described in Chapter 5 of this Visioning Study.
 - There is opportunity for a secondary trail entry south Stearns Street which can add to the overall sense of entry and ease of connectivity.
- **Address street crossing issues**
 - A railroad crossing constraint exists between Hidden Ranch Drive and Stearns Street, and a direct trail connection is very difficult if not impossible. Additional studies are recommended in this area to develop the best trail alignment.

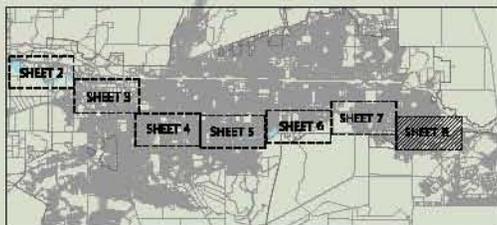
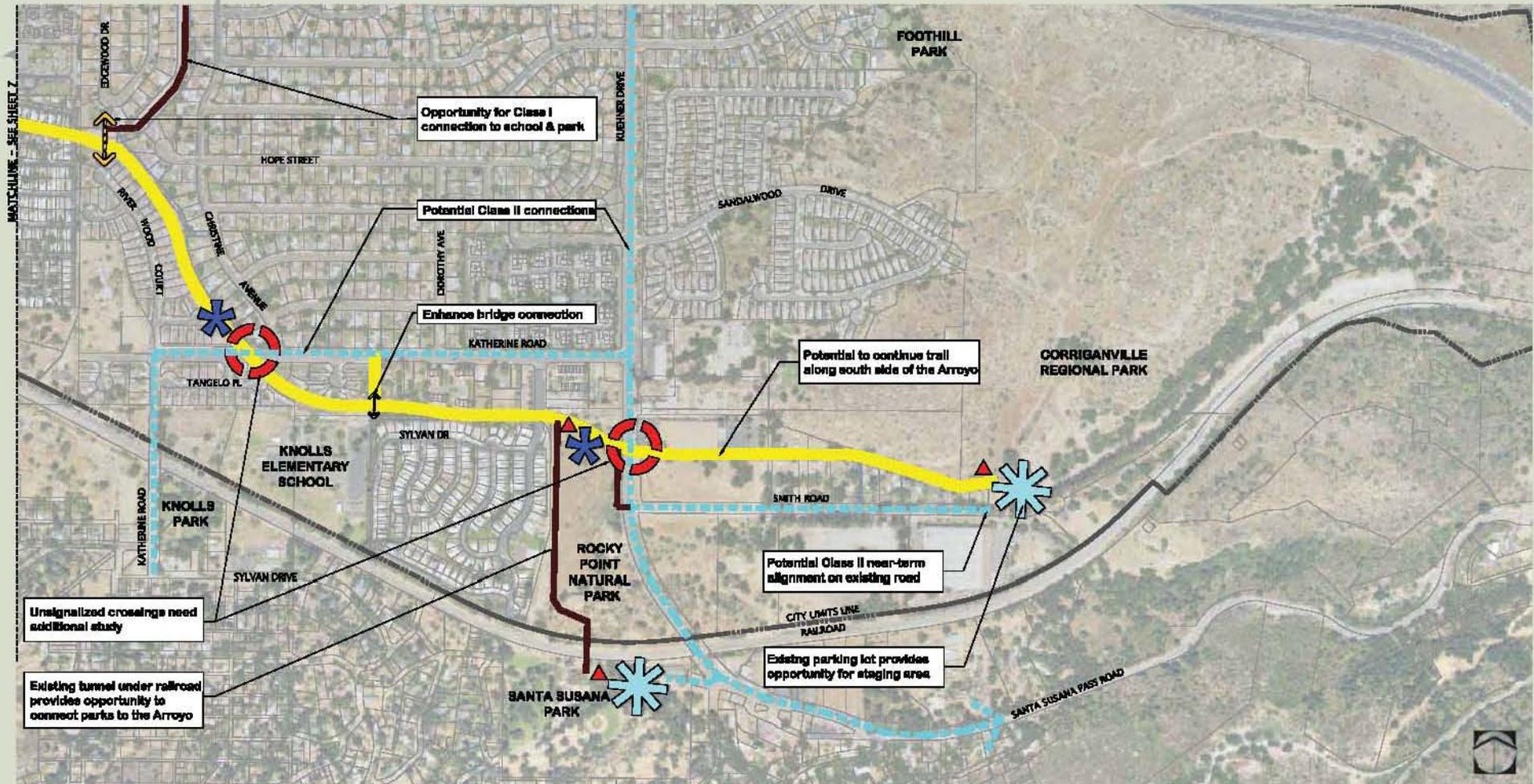
- The unsignalized street crossings in this segment which are safety constraints to trail users, and additional studies are recommended. Unsignalized crossings should incorporate the street crossing enhancements described in Chapter 5 of this Visioning Study:
 - Stow Street;
 - Yosemite Avenue;
 - Katherine Road;
 - Kuehner Drive



LEGEND

	Existing At Grade Crossing		Proposed Screening/Enhancement
	Existing Bridge Undercrossing		High Priority Habitat Enhancement
	Proposed Pocket Park		Primary Trail Alignment
	Proposed Overlook/Rest Area		Secondary Trail Alignment
	Proposed Bridge		Connector Trail Alignment
	Existing Bridge		Existing Regional Trail
	Proposed Trailhead/Parking		Ultimate Class II or III Bike Lane
	Proposed Gateway		Proposed Interpretive Exhibit

Conceptual Vision Plan
Segment C
Sheet 7



LEGEND

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| | Existing At-Grade Crossing | | Proposed Screening/Enhancement |
| | Existing Bridge Undercrossing | | High Priority Habitat Enhancement |
| | Proposed Pocket Park | | Primary Trail Alignment |
| | Proposed Overlook/Rest Area | | Secondary Trail Alignment |
| | Proposed Bridge | | Connector Trail Alignment |
| | Existing Bridge | | Existing Regional Trail |
| | Proposed Trailhead/Parking | | Ultimate Class II or III Side Lane |
| | Proposed Gateway | | Proposed Interpretive Exhibit |

Conceptual Vision Plan
Segment C
Sheet 8